

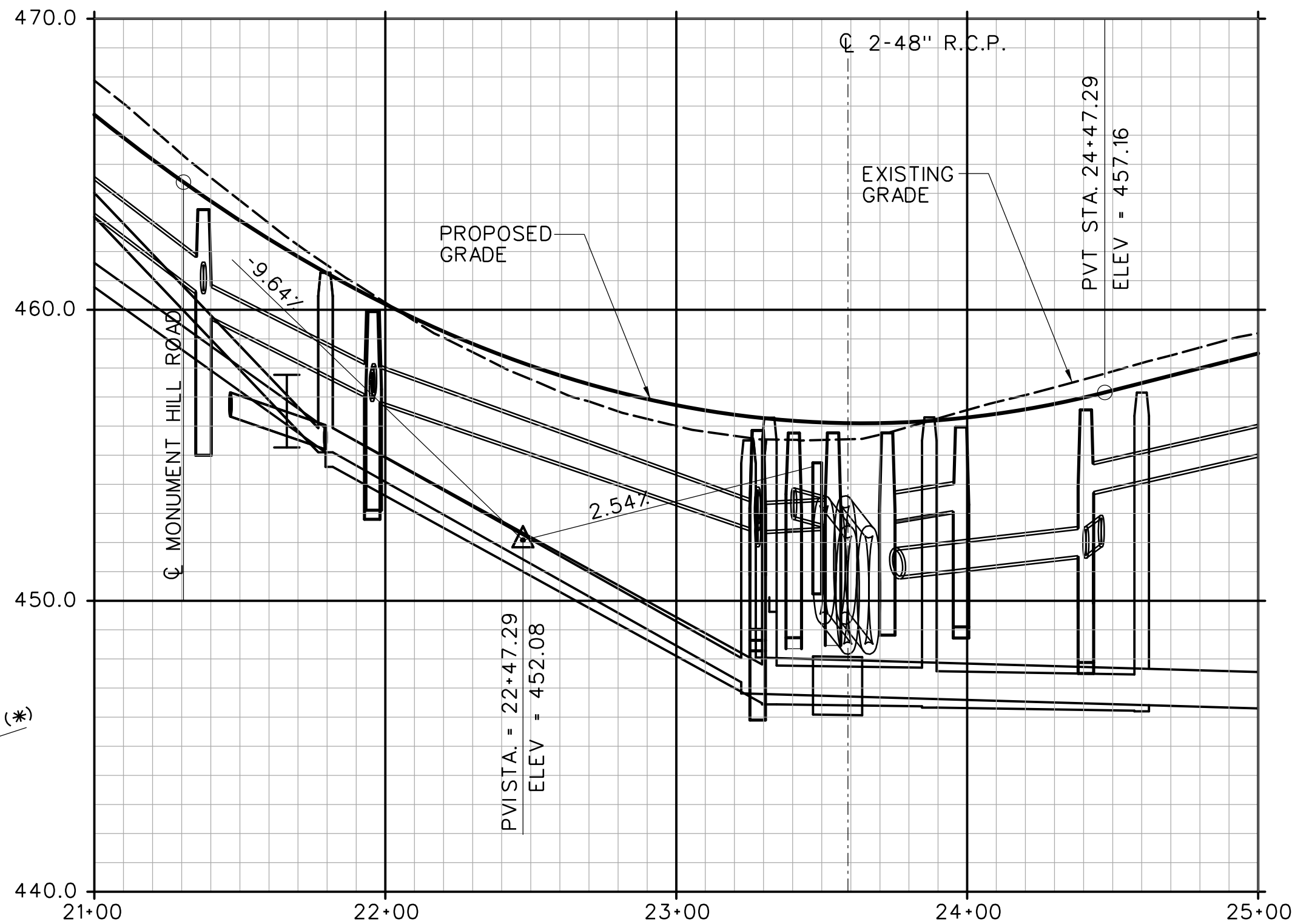
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SCALE: 1"=40'-0" H  
SCALE: 1"=4'-0" V

VERTICAL CURVE DATA  
P.V.C. 20+47.29, EL. 471.36  
P.V.I. 22+47.29, EL. 452.68  
P.V.T. 24+47.29, EL. 457.16  
LVC = 400'

HYDRAULIC DATA (48" R.C. PIPES)	
DRAINAGE AREA	0.15 SQ. MILE
DESIGN FREQUENCY	50 YEAR
DESIGN DISCHARGE	137 CFS
AVERAGE DAILY FLOW ELEVATION*	450.04 FT. OBSERVED ON 6/8/99
UPSTREAM DESIGN WATER SURFACE ELEVATION	454.6 FT.
DOWNSTREAM DESIGN WATER SURFACE ELEVATION	451.9 FT.

WORKING POINT COORDINATES		
W.P.#	NORTH	EAST
1	841100.62	1122065.07
2	841058.72	1122069.73
3	841134.38	1122130.73
4	841078.46	1122129.87
5	841046.07	1122129.38
6	841029.40	1122129.12
7	840907.53	1122152.04
8	840992.31	1122074.04

LAYOUT PLAN  
SCALE: 1/8"=1'-0"

(\*) - LAYOUT OF METAL BRIDGE RAIL/CHAIN LINK FENCE POST MEASURED HORIZONTALLY IN PLAN

GENERAL NOTES CONTD.

STAGED CONSTRUCTION: TRAFFIC SHALL BE MAINTAINED AT ALL TIMES IN ACCORDANCE WITH THE PROVISIONS FOR "MAINTENANCE AND PROTECTION OF TRAFFIC" AND "PROSECUTION AND PROGRESS". THE WORK WILL BE STAGED TO PROVIDE UNINTERRUPTED SERVICE FOR ROADWAY TRAFFIC ON ROUTE 31 AND TO MAINTAIN WATER FLOWS FOR MILL BROOK. THE INFORMATION SHOWN ON THE STAGED CONSTRUCTION AND WATER HANDLING, DRAWINGS STR-4 AND STR-5 DEPICTS "SUGGESTED METHODS OF CONSTRUCTION" AND IS INTENDED TO CONVEY THE ASSUMPTIONS MADE BY THE DESIGNER TO DEVELOP THE REQUIREMENTS FOR REGULATORY PERMITS INCLUDED WITH THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SELECTING MEANS AND METHODS FOR THE SAFE AND SUCCESSFUL COMPLETION OF CONSTRUCTION. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT WORKING DRAWINGS TO THE ENGINEER FOR APPROVAL IN ACCORDANCE WITH ARTICLE 1.05.02. THESE DRAWINGS SHALL INCLUDE HIS PROPOSED METHODS OF CONSTRUCTION, INCLUDING BUT NOT LIMITED TO SEQUENCING AND COMPLETE DETAILS OF MATERIALS, METHODS AND EQUIPMENT TO BE USED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING REVISED PERMITS FROM ALL GOVERNING LOCAL, STATE AND FEDERAL AGENCIES DUE TO HIS PROPOSED CHANGES IN CONSTRUCTION METHODOLOGY, WHICH EFFECT ENVIRONMENTAL IMPACTS.

CONCRETE NOTES:

CLASS "A" CONCRETE: CLASS "A" CONCRETE SHALL BE USED FOR ALL CAST-IN-PLACE COMPONENTS.

JOINT SEAL: SEE SPECIAL PROVISIONS.

EXPOSED EDGES: EXPOSED EDGES OF CONCRETE SHALL BE BEVELED 1" X 1" UNLESS DIMENSIONED OTHERWISE.



CONCRETE COVER: ALL REINFORCEMENT SHALL HAVE 2"  
COVER UNLESS DIMENSIONED OTHERWISE.

REINFORCEMENT: ALL REINFORCEMENT SHALL BE ASTM A615  
GRADE 60 UNLESS NOTED OTHERWISE.

PREFORMED EXPANSION JOINT FILLER: THE COST OF FURNISHING AND INSTALLING PREFORMED EXPANSION JOINT FILLER SHALL BE INCLUDED IN THE COST OF THE ITEM "CLASS 'A' CONCRETE".

CONSTRUCTION JOINTS: CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN ON THE PLANS, WILL NOT BE PERMITTED WITHOUT PRIOR APPROVAL OF THE ENGINEER.

SIMULATED STONE MASONRY: SEE SPECIAL PROVISIONS

				THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		DESIGNER/DRAFTER: L.PIPPIN/J.GOPAL CHECKED BY: L.PIPPIN SCALE AS NOTED	 <b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION		PROJECT TITLE:  <b>RECONSTRUCTION OF ROUTE 31</b>	TOWN:  <b>COVENTRY</b>	PROJECT NO. <b>32-130</b> DRAWING NO. <b>STR-03</b> SHEET NO. <b>04.03</b>
REV.	DATE	REVISION DESCRIPTION		SHEET NO.		ENGINEER: BSC GROUP / BY GM2 ASSOCIATES, INC.		APPROVED BY:		DATE:	
Filename:											

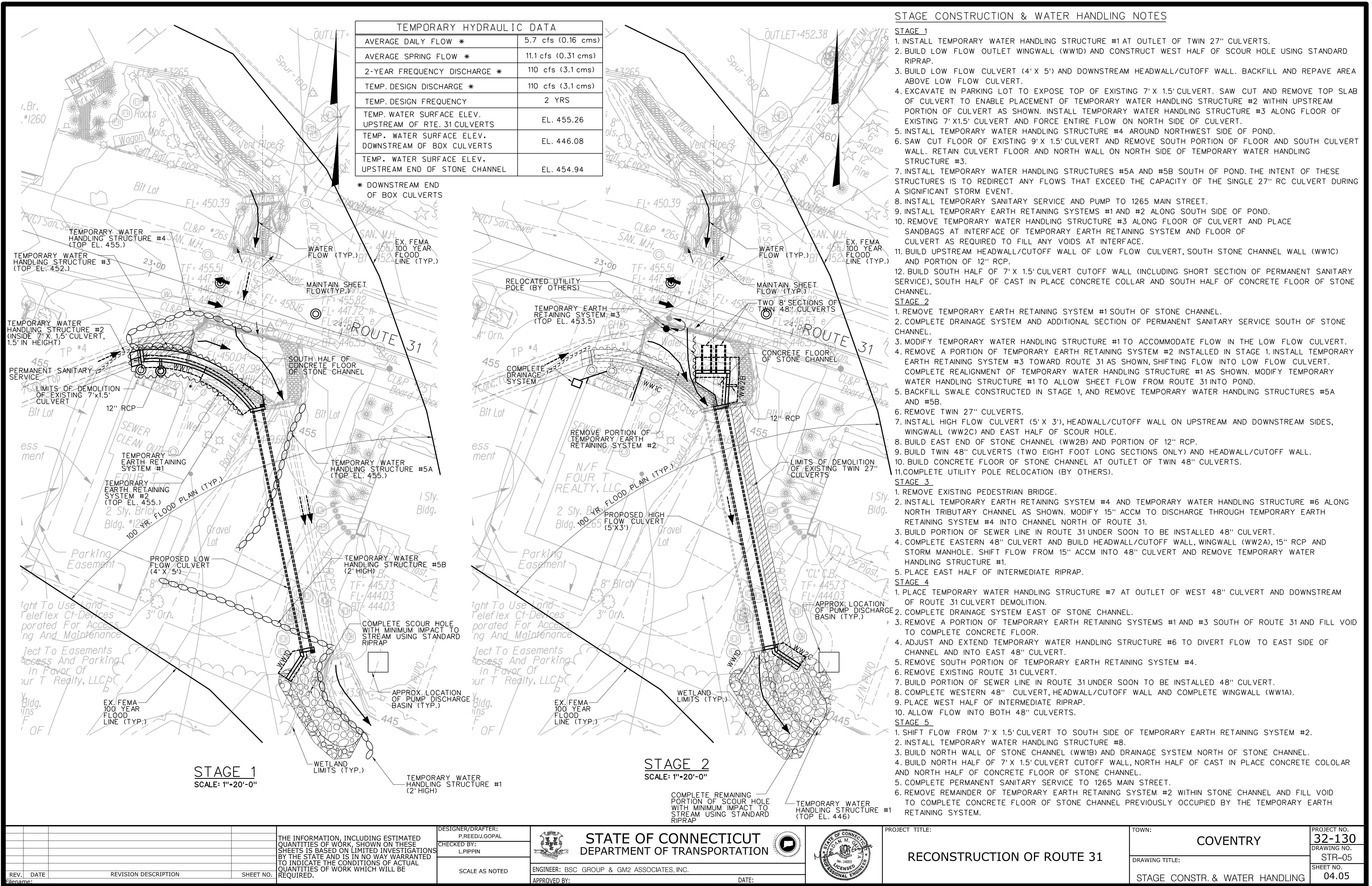


Driller:	M. McDonough	Connecticut DOT Boring Report	Hole No.:	B-1				
Inspector:	R. Pion	Town:	COVENTRY	Stat./Offset:24+09 158 ft Rt				
Engineer:	R. Pion	Project No.:	32-130	Northing: 840912				
Start Date:	11/20/2006	Route No.:	31	Easting: 1122125				
Finish Date:	11/20/2006	Bridge No.:		Surface Elevation: 451.4				
Project Description: Route 31 Safety Improvements								
Casing Size/Type:	4" HW	Sampler Type/Size:	SS 1 3/8 in	Core Barrel Type:2" NX				
Hammer Wt.300 lbs Fall:24"		Hammer Wt.:140 lbs	Fall: 30 in					
Groundwater Observations @4 after0 hours, @ after hours, @ after hours								
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)
0						GLACIAL TILL		450
5	S-1	4 4 28 10	24	4			Brown c to f GRAVEL, and c to f SAND, trace silt, with cobbles	445
10	S-2	25 25 27 26	24	11			Brown c to f SAND, and f to c GRAVEL, trace silt, with cobbles	440
15	C-1		60	60	46	WEATHERED BEDROCK	Gray moderately fractured slightly weathered schist Core times (in min/ft): 8,8,10,12,15	435
20	C-2		60	60	79		Gray slightly fractured slightly weathered schist Core times (in min/ft): 18,10,12,12,12	430
25							Bottom of Boring	425
Sample Type: S=Split Spoon C=Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%								
Total Penetration in Earth:13.5 Rock: 11.5		NOTES: Drilled through weathered rock with rollerbit from 13.5 to 15 ft.					Sheet 1 of 1	
No. of Samples:2							SM-001-M REV. 1/02	

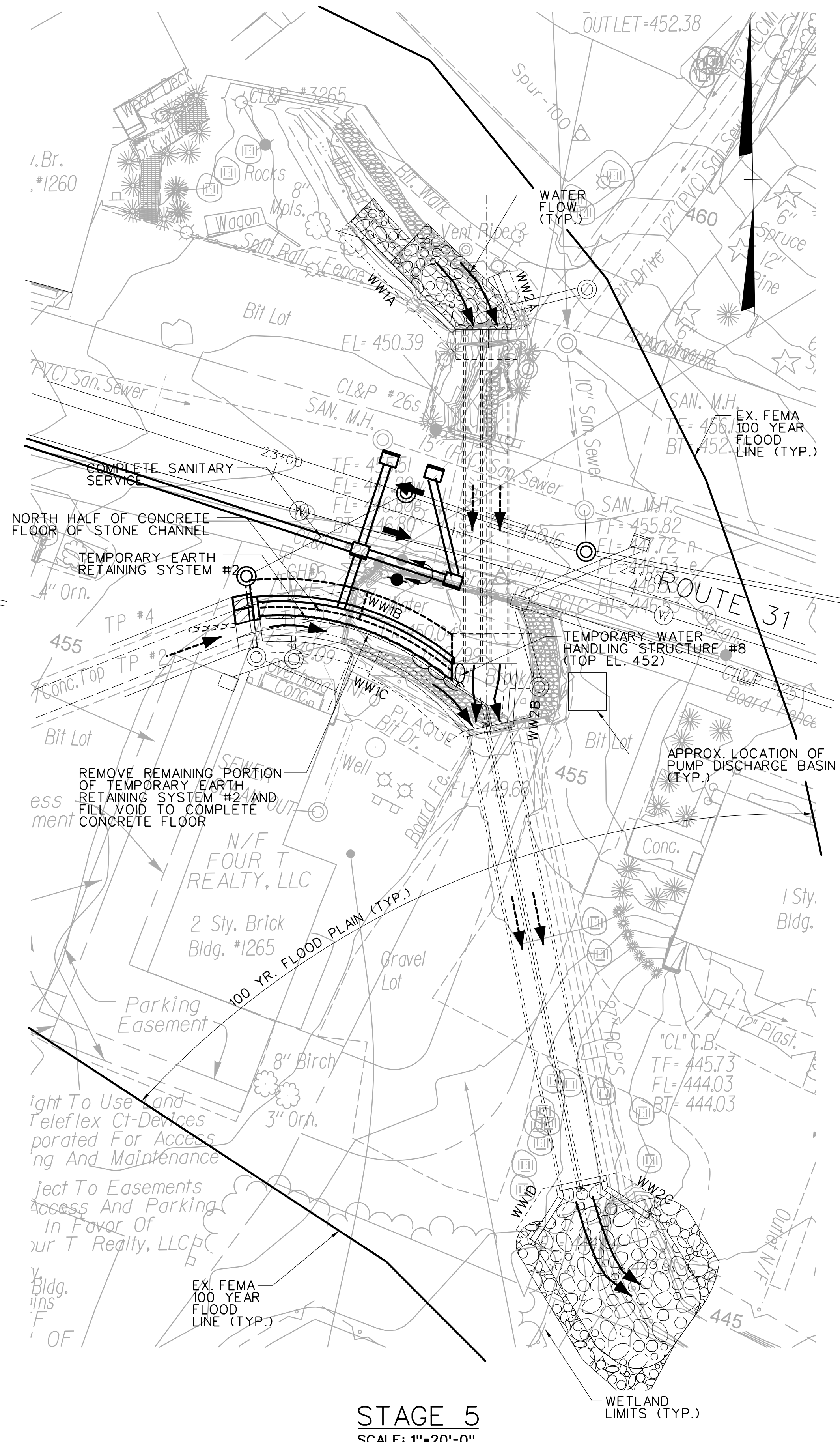
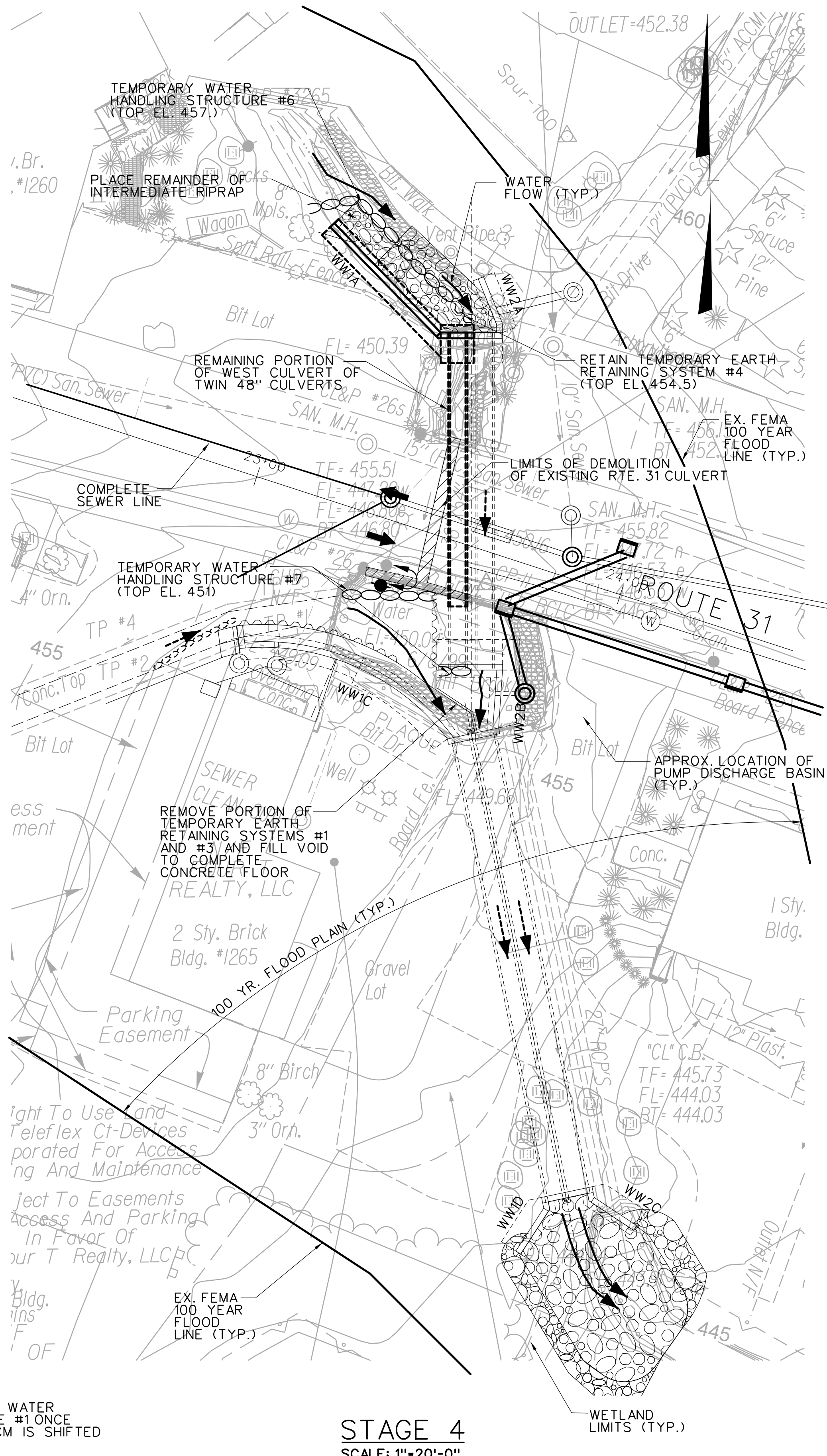
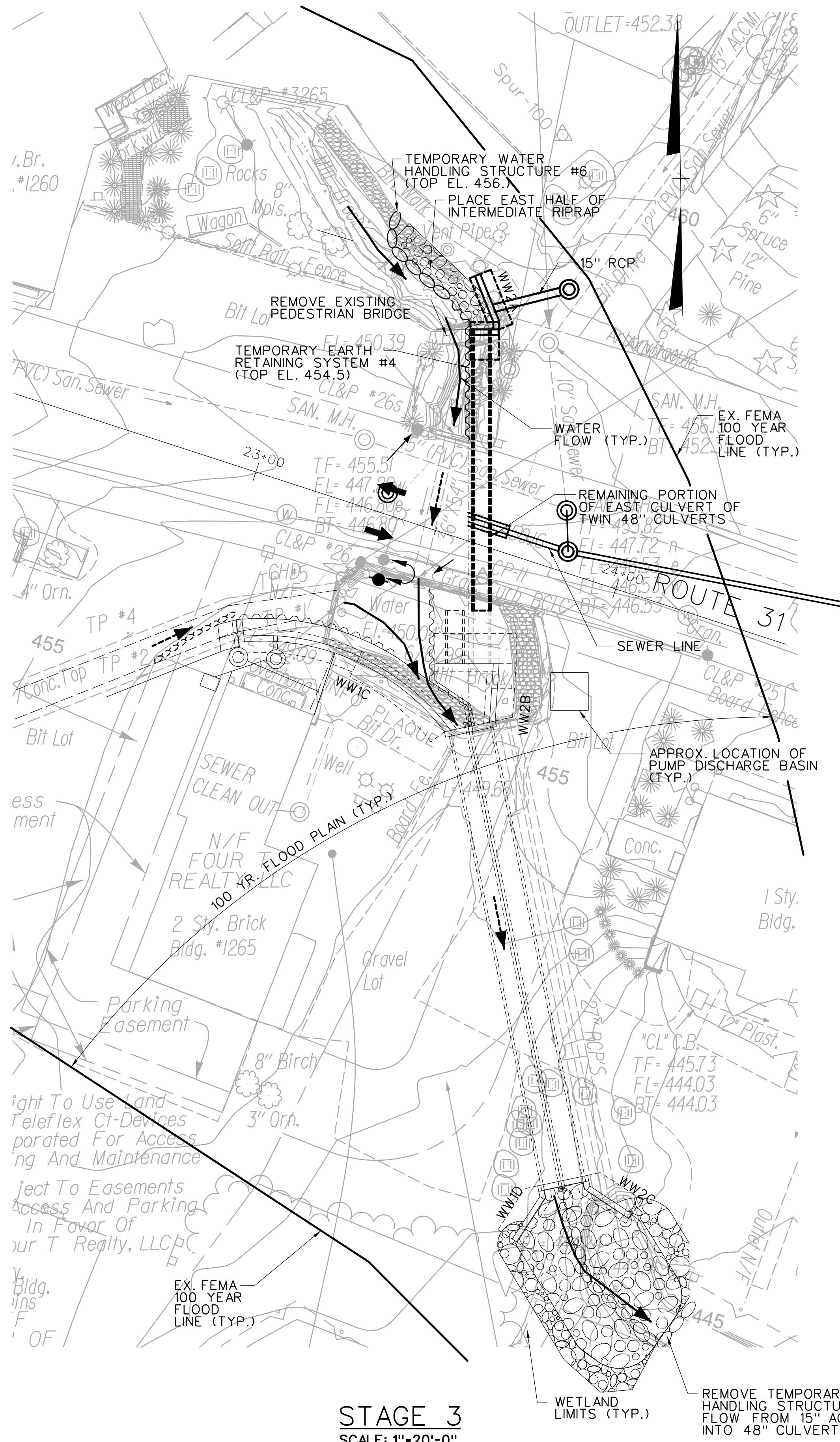
Driller:	M. McDonough	Connecticut DOT Boring Report	Hole No.:	B-2				
Inspector:	R. Pion	Town:	COVENTRY	Stat./Offset:23+26 23 ft Lft.				
Engineer:	R. Pion	Project No.:	32-130	Northing: 841115				
Start Date:	11/20/2006	Route No.:	31	Easting: 1122110				
Finish Date:	11/20/2006	Bridge No.:		Surface Elevation: 456.1				
Project Description: Route 31 Safety Improvements								
Casing Size/Type:	3" NW	Sampler Type/Size:	SS 1 3/8 in	Core Barrel Type:2" NX				
Hammer Wt.300 lbs Fall:24"		Hammer Wt.:140 lbs	Fall: 30 in					
Groundwater Observations @5 after0 hours, @ after hours, @ after hours								
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)
0						PAVEMENT STRUCTURE/ GLACIAL TILL		455
5	S-1	3 15 10 11	24	9			Brown c to f GRAVEL, some f to c sand, some silt, with cobbles	450
10	S-2	50	0	0		BOULDER GLACIAL TILL	Brown c to f GRAVEL, some f to c sand, some silt, with cobbles	445
	S-3	146	6	4		WEATHERED BEDROCK	Brown c to f GRAVEL, some f to c sand, some silt, with cobbles	
15	C-1		60	60	8	BEDROCK	Gray slightly weathered moderately fractured schist Core times (in min/ft): 5,5,8,8,7	440
20	C-2		60	60	38		Gray slightly weathered highly fractured schist Core times (in min/ft): 8,8,7,7,6	435
25							Bottom of Boring	430
Sample Type: S=Split Spoon C=Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%								
Total Penetration in Earth:16 Rock: 10		NOTES: Drilled through weathered rock with roller bit from 12 to 16 feet.					Sheet 1 of 1	
No. of Samples:3							SM-001-M REV. 1/02	

Driller:	M. McDonough	Connecticut DOT Boring Report	Hole No.:	B-3				
Inspector:	R. Pion	Town:	COVENTRY	Stat./Offset:23+96 22 ft Rt.				
Engineer:	R. Pion	Project No.:	32-130	Northing: 841045				
Start Date:	11/20/2006	Route No.:	31	Easting: 1122161				
Finish Date:	11/20/2006	Bridge No.:		Surface Elevation: 456.1				
Project Description: Route 31 Safety Improvements								
Casing Size/Type:	3" NW	Sampler Type/Size:	SS 1 3/8 in	Core Barrel Type:NX				
Hammer Wt.300 lbs Fall:24"		Hammer Wt.:140 lbs	Fall: 30 in					
Groundwater Observations @4 after0 hours, @ after hours, @ after hours								
Depth (ft)	Sample Type/No.	Blows on Sampler per 6 inches	Pen. (in.)	Rec. (in.)	RQD %	Generalized Strata Description	Material Description and Notes	Elevation (ft)
0						PAVEMENT STRUCTURE/ GLACIAL TILL		455
5	S-1	2 2 3 4	24	12			Brown f to c SAND, and f to c GRAVEL, trace silt	450
10	S-2	10 9 10 11	24	14			Gray f to c SAND, and f to c GRAVEL, trace silt	445
15	S-3	50	0	0		WEATHERED BEDROCK	Weathered bedrock	440
	C-1		60	48	0	BEDROCK	Gray moderately fractured slightly weathered schist Core times (in min/ft): 5,5,5,7,7	
20	C-2		60	60	45		Gray moderately fractured slightly weathered schist Core times (in min/ft): 8,8,8,7,8	435
25							Bottom of Boring	430
Sample Type: S=Split Spoon C=Core UP = Undisturbed Piston V = Vane Shear Test Proportions Used: Trace = 1 - 10%, Little = 10 - 20%, Some = 20 - 35%, And = 35 - 50%								
Total Penetration in Earth:16 Rock: 10		NOTES: Rollerbitted into weathered rock from 13.5 to 16 feet					Sheet 1 of 1	
No. of Samples:3							SM-001-M REV. 1/02	



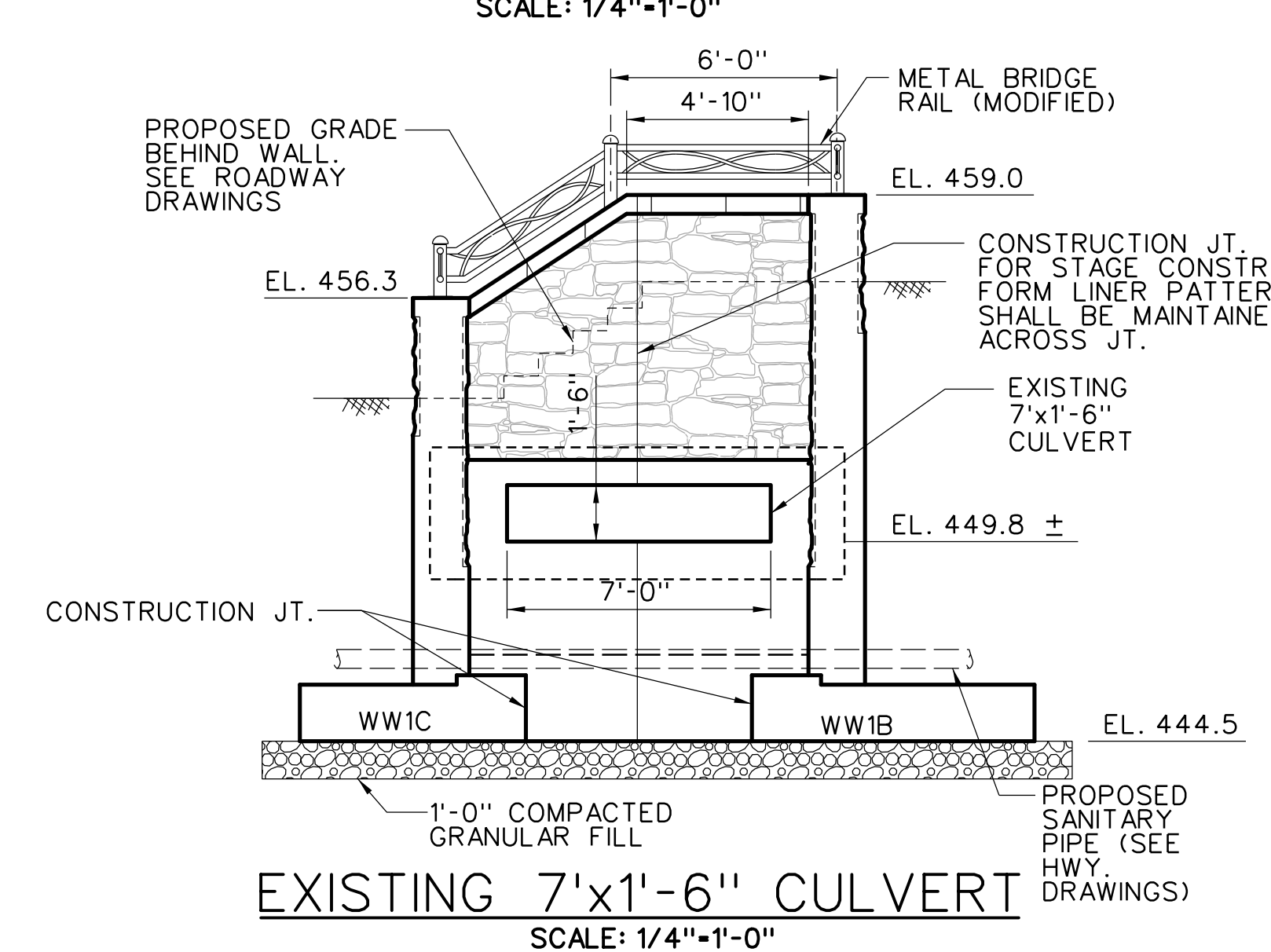
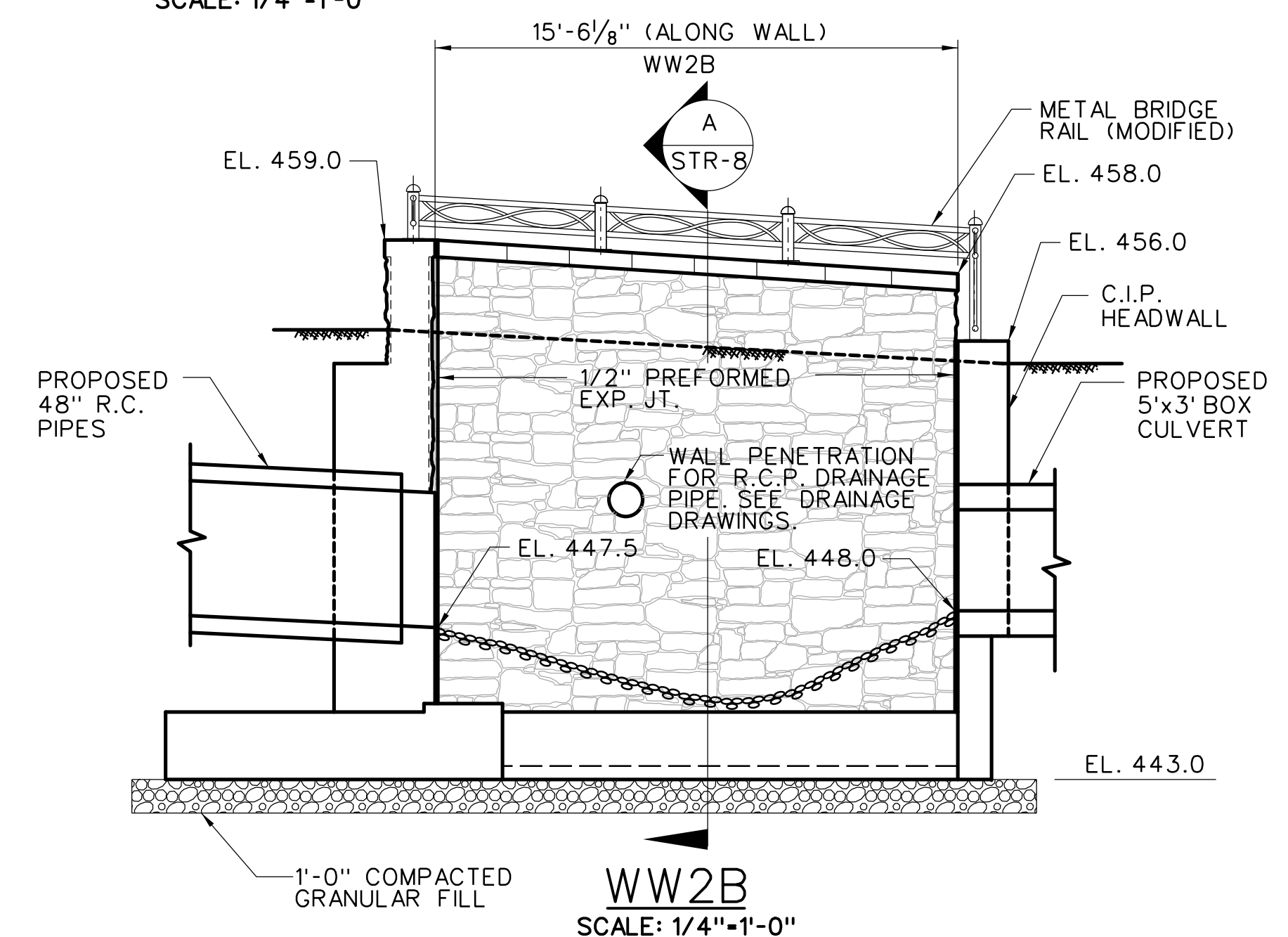
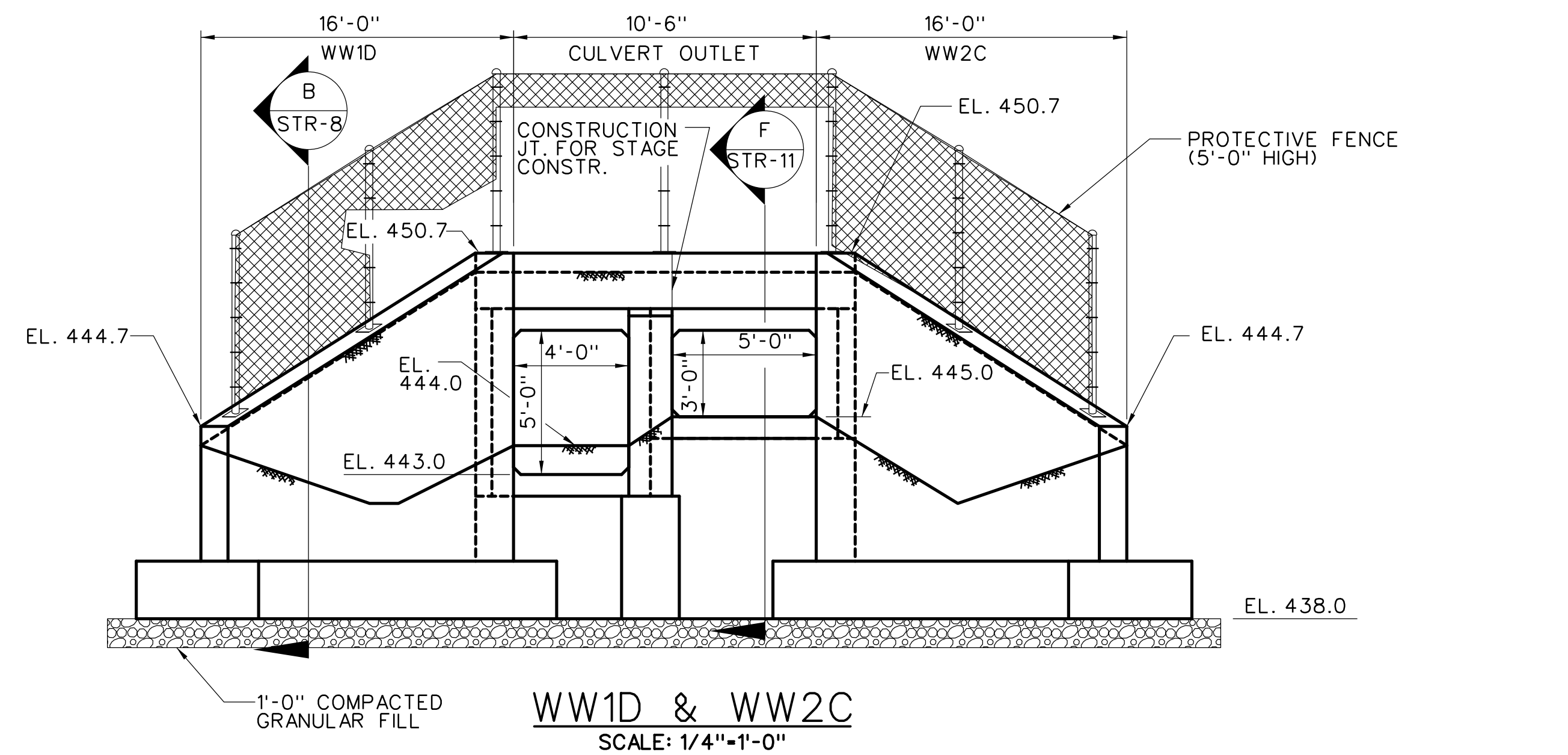
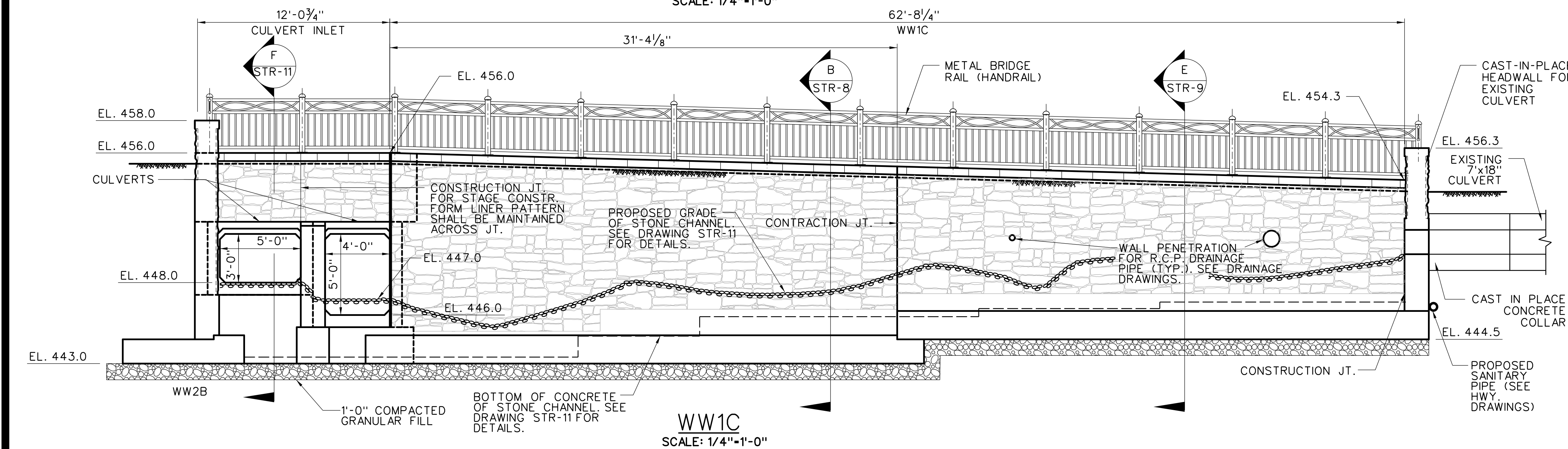
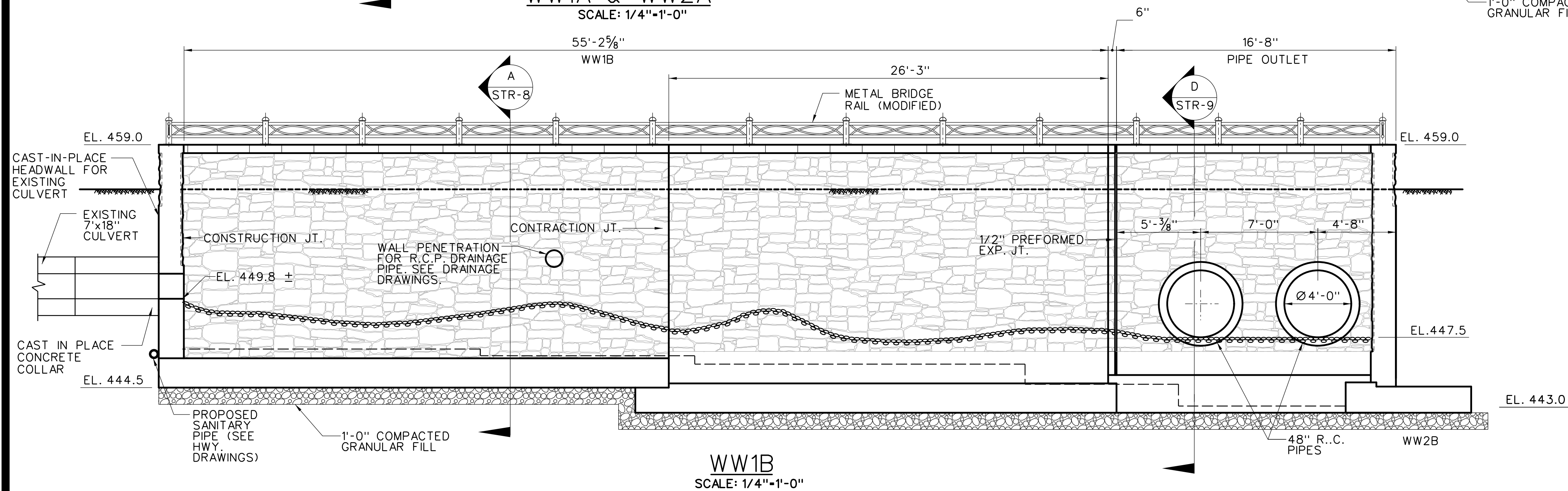
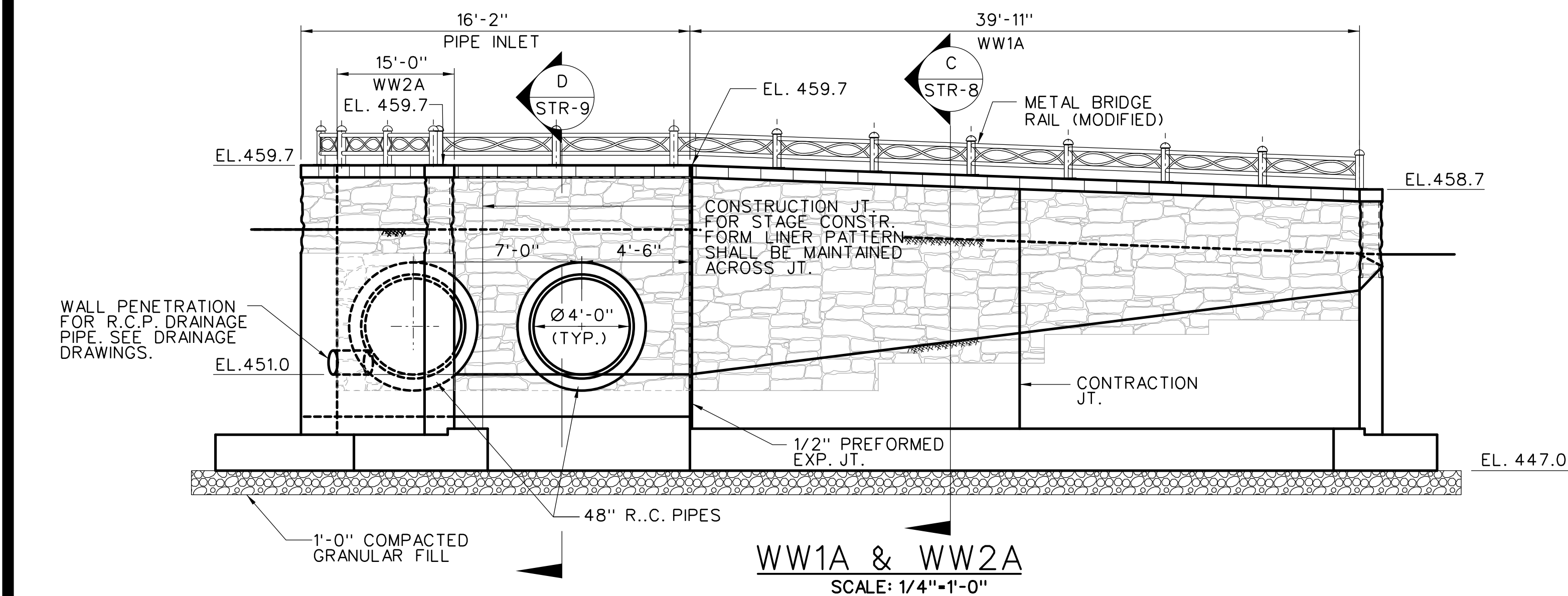






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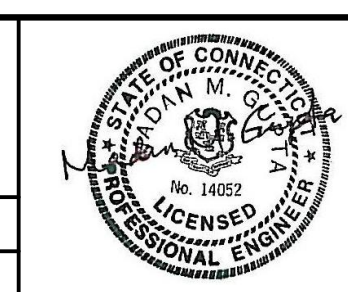


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THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DESIGNER/DRAFTER:  
L.PIPIN/J.GOPAL  
CHECKED BY:  
L.PIPIN  
SCALE AS NOTED

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION  
ENGINEER: BSC GROUP / BY GM2 ASSOCIATES, INC.  
APPROVED BY: DATE:



PROJECT TITLE:  
**RECONSTRUCTION OF ROUTE 31**

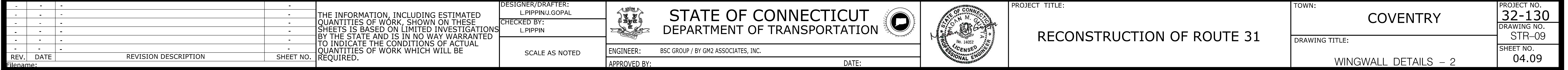
TOWN:  
**COVENTRY**  
DRAWING TITLE:  
**WINGWALL ELEVATIONS**

PROJECT NO.  
**32-130**  
DRAWING NO.  
**STR-07**  
SHEET NO.  
**04.07**

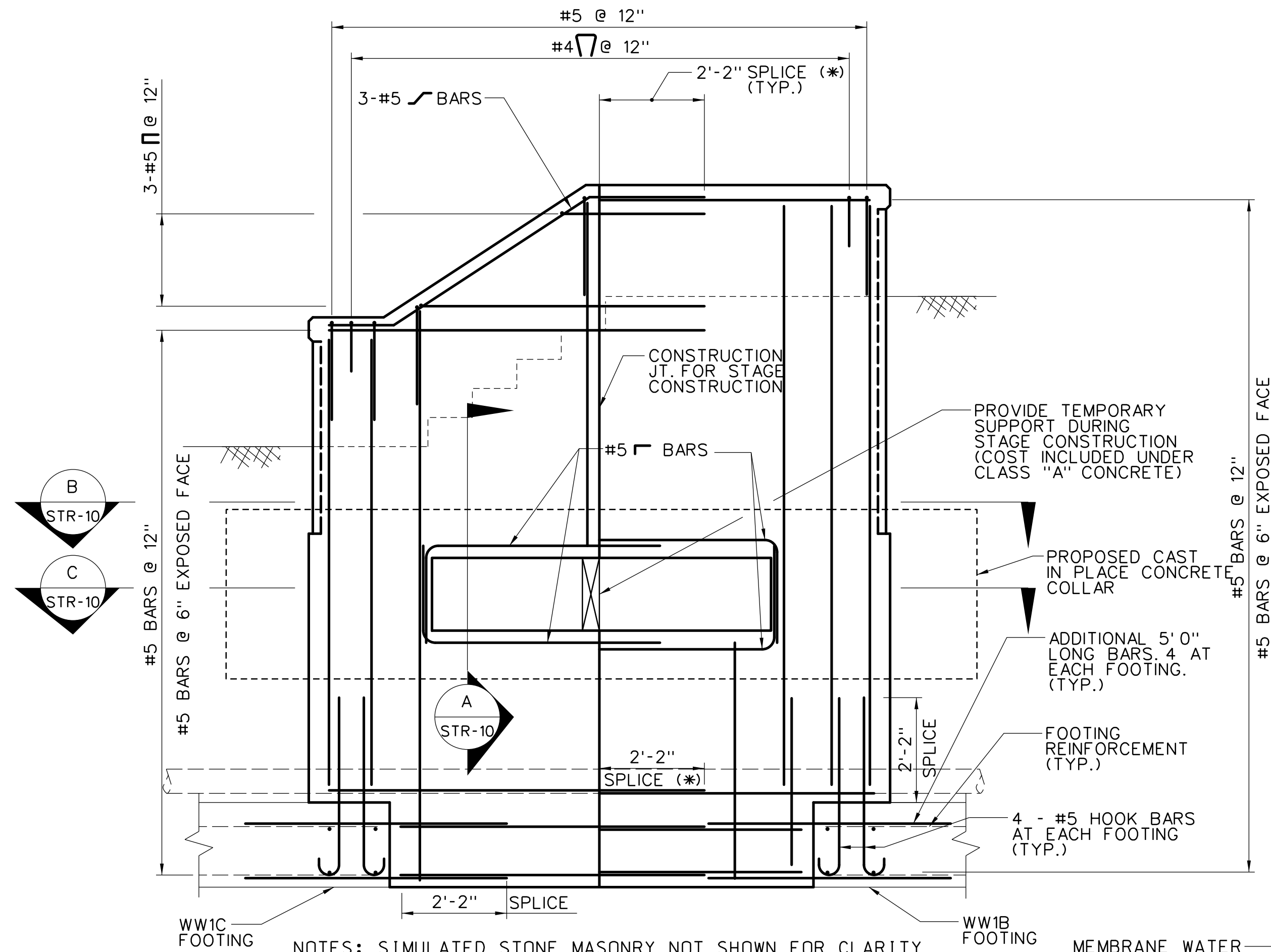






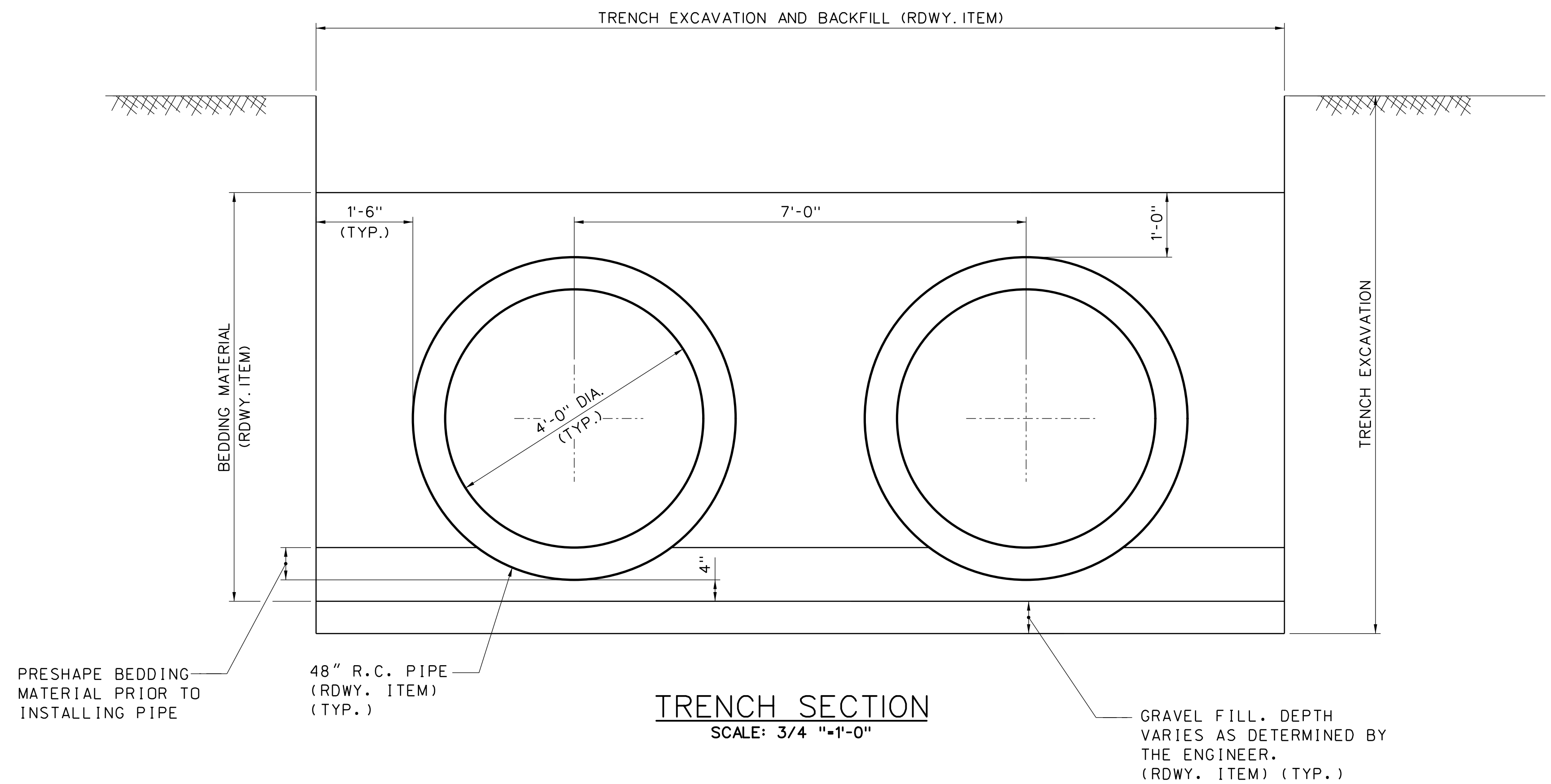




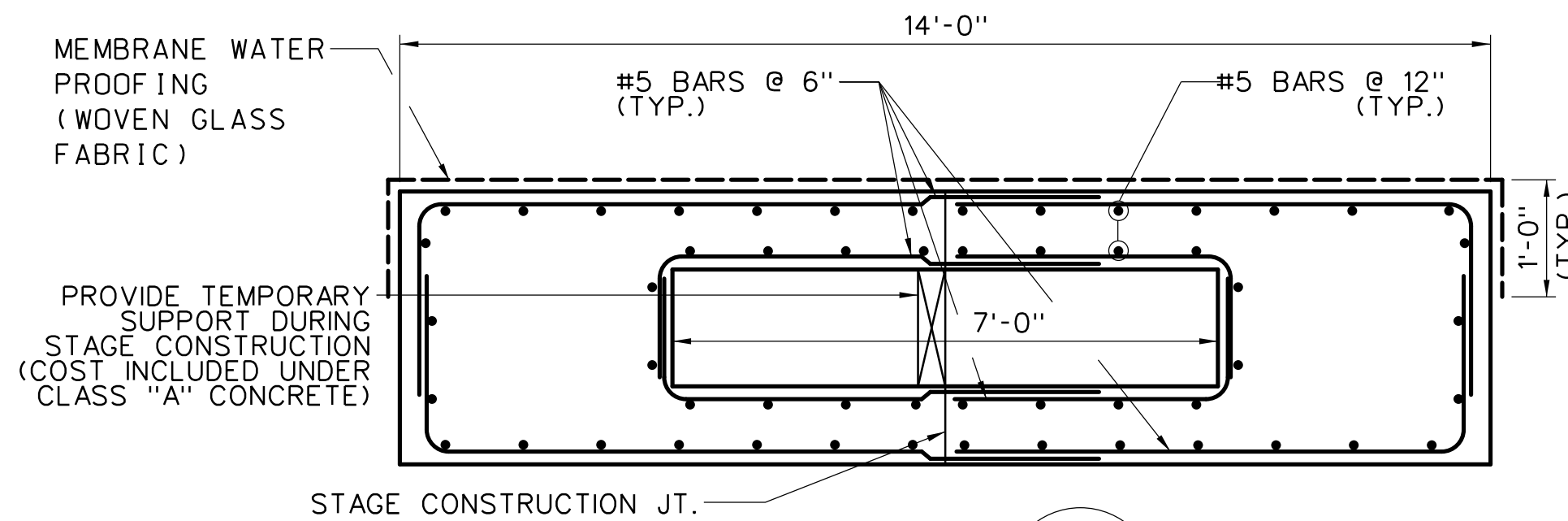


NOTES: SIMULATED STONE MASONRY NOT SHOWN FOR CLARITY  
NO BACKFILLING IS PERMITTED UNTIL THE ENTIRE WALL IS CONSTRUCTED  
\* - THE CONTRACTOR MAY USE MECHANICAL REINFORCING BAR SPLICERS INSTEAD OF TENSION LAP SPLICES, HOWEVER NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR THE USE OF MECHANICAL REINFORCING BAR SPLICERS.

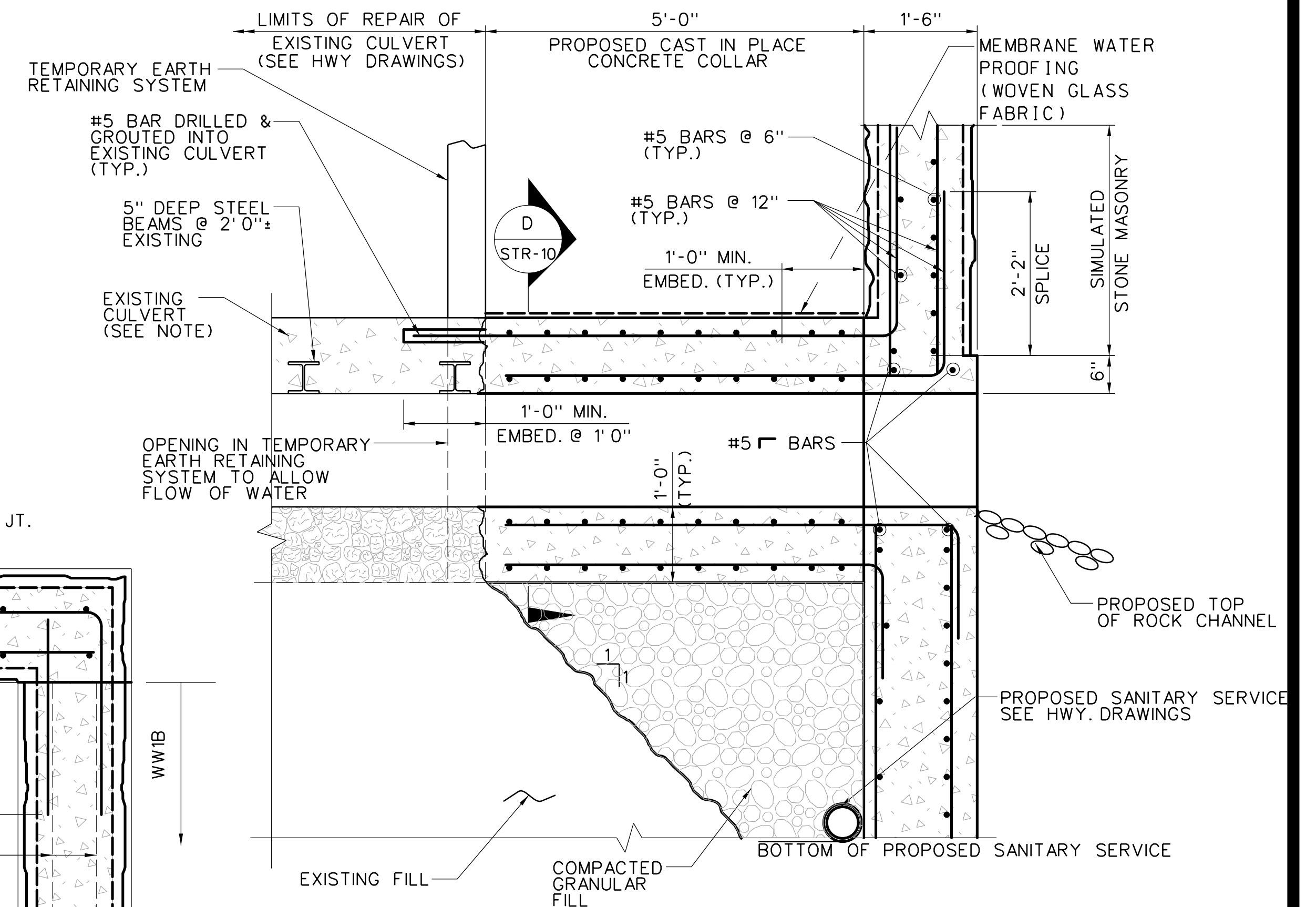
**WALL SECTION AT 7'x1'-6" CULVERT**  
SCALE: 1/2 " = 1'-0"



**TRENCH SECTION**  
SCALE: 3/4 " = 1'-0"

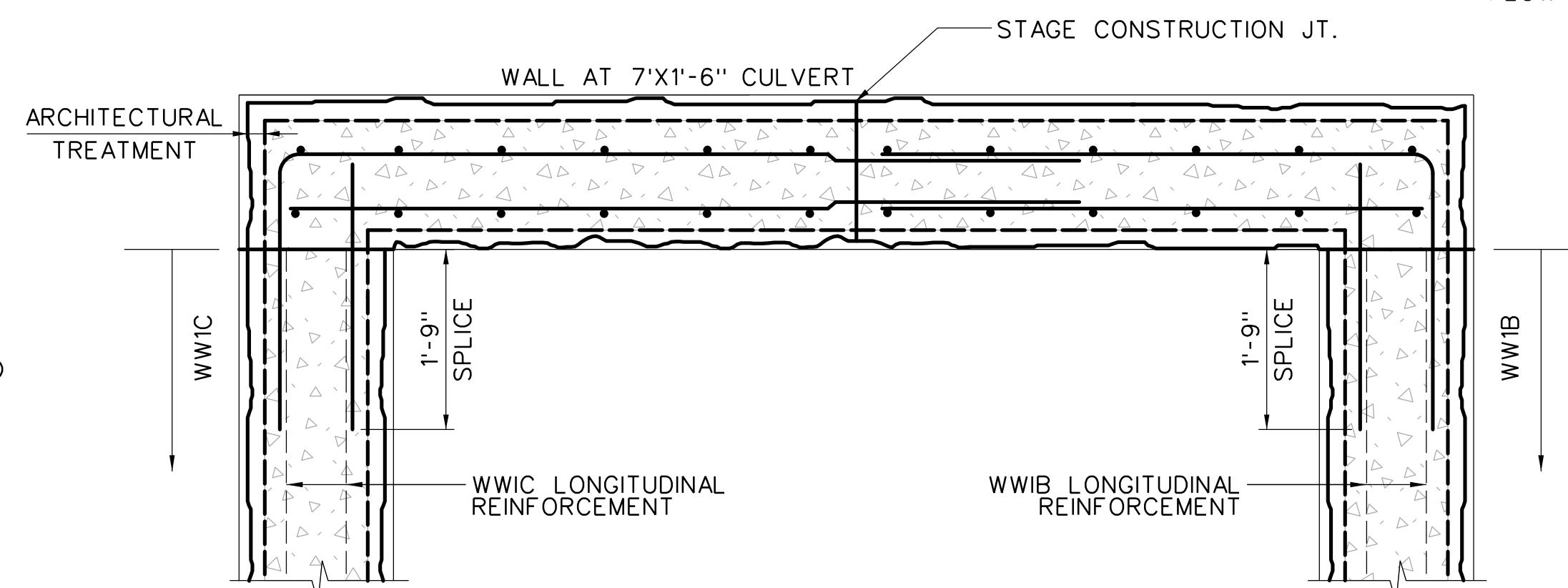


**SECTION D**  
SCALE: 1/2 " = 1'-0" STR-10

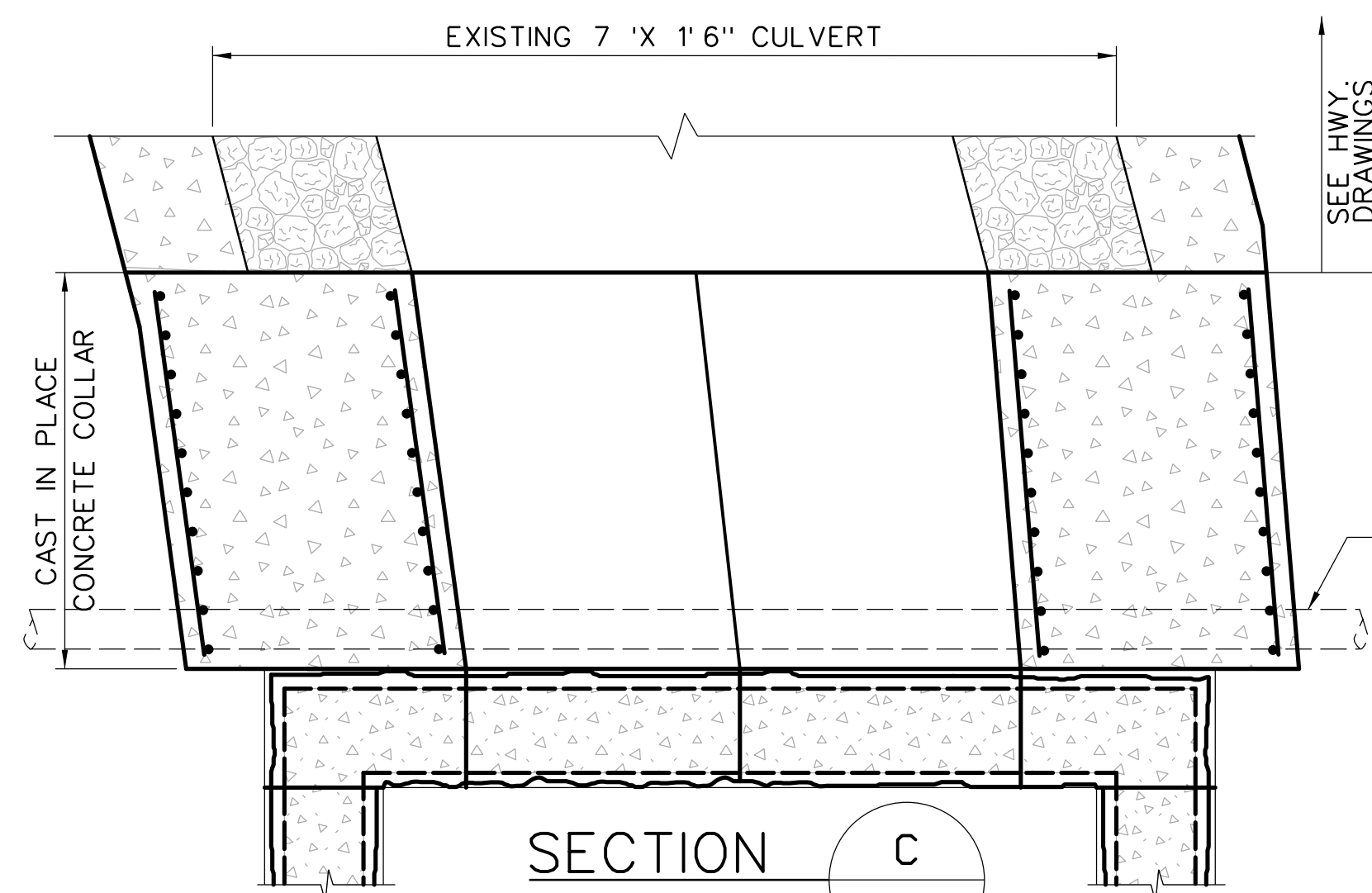


**SECTION A**  
SCALE: 3/4 " = 1'-0" STR-10

NOTE:  
THE DETAILS OF THE EXISTING 7'-0" X 1'-6" CULVERT SHOWN ON THESE DRAWINGS ARE BASED ON LIMITED INFORMATION OBTAINED FROM THE SURVEY AND VISUAL FIELD INSPECTIONS. THE CONTRACTOR SHALL DETERMINE THE EXACT NATURE OF THE CULVERT DURING CONSTRUCTION AND SHALL INFORM THE ENGINEER IMMEDIATELY IN THE EVENT THAT THE CULVERT DOES NOT MATCH WHAT IS SHOWN ON THESE DRAWINGS.



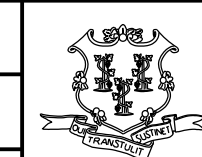
**SECTION B**  
SCALE: 3/4 " = 1'-0" STR-10



**SECTION C**  
SCALE: 1/2 " = 1'-0" STR-10

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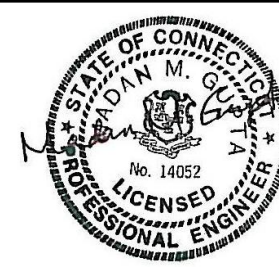
DESIGNER/DRAFTER: L.PIPPIN/J.GOPAL
CHECKED BY: L.PIPPIN
SCALE AS NOTED



**STATE OF CONNECTICUT**  
DEPARTMENT OF TRANSPORTATION



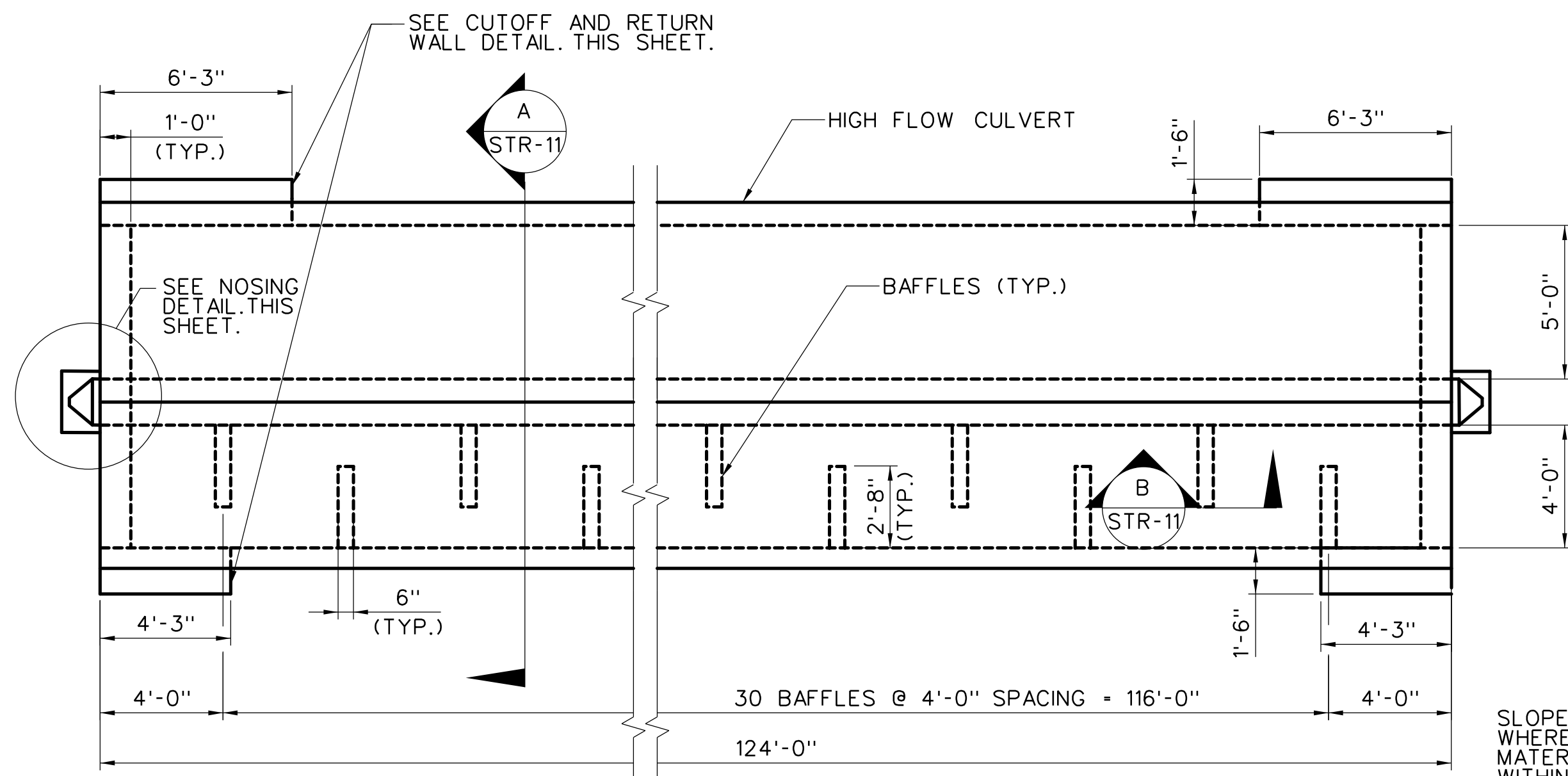
ENGINEER: BSC GROUP / BY GM2 ASSOCIATES, INC.	DATE:
APPROVED BY:	



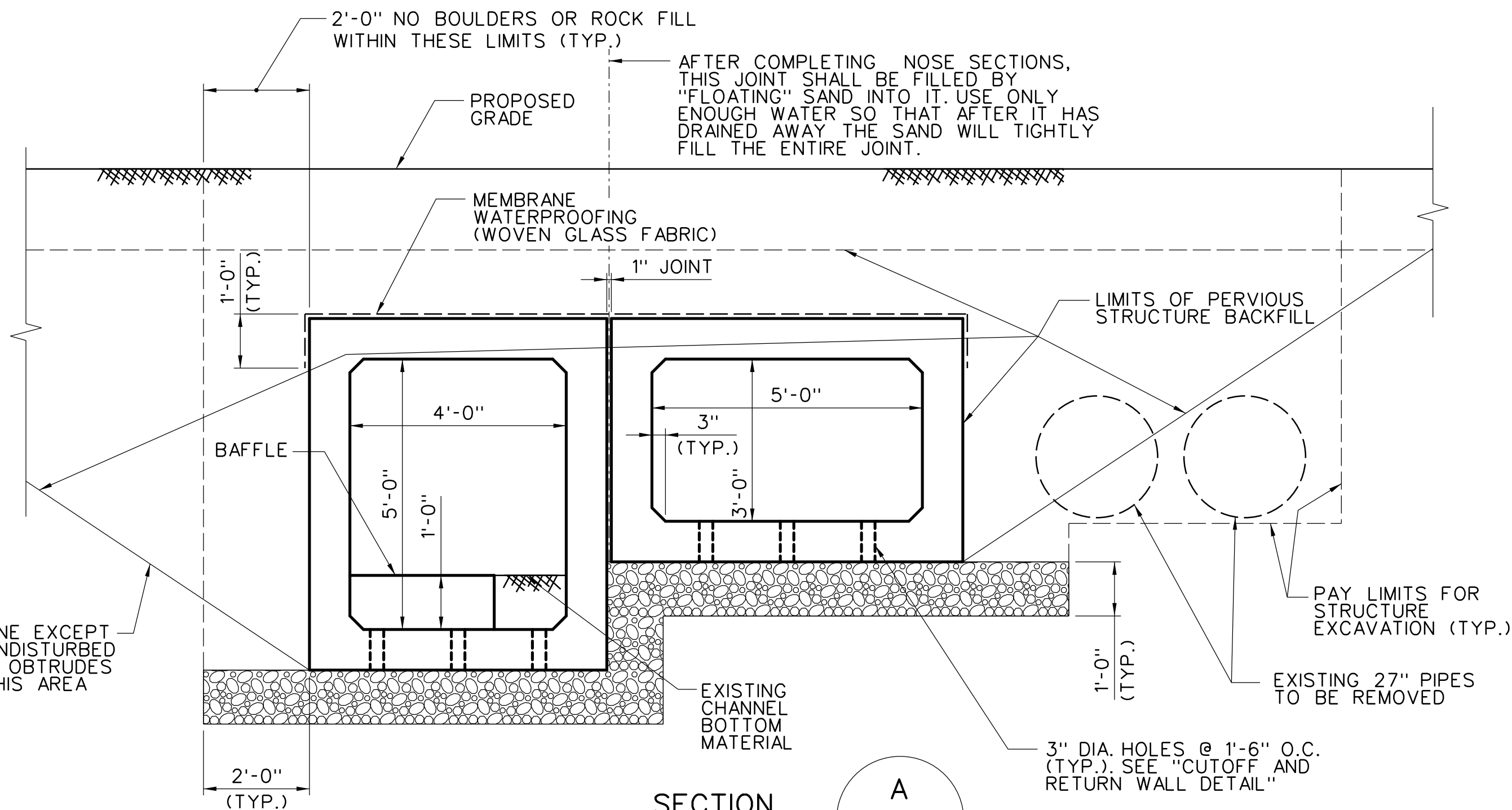
PROJECT TITLE:  <b>RECONSTRUCTION OF ROUTE 31</b>
---

TOWN:  <b>COVENTRY</b>	PROJECT NO. <b>32-130</b>
DRAWING TITLE:  WINGWALL DETAILS - 3	DRAWING NO. <b>STR-10</b>
	SHEET NO. <b>04.10</b>

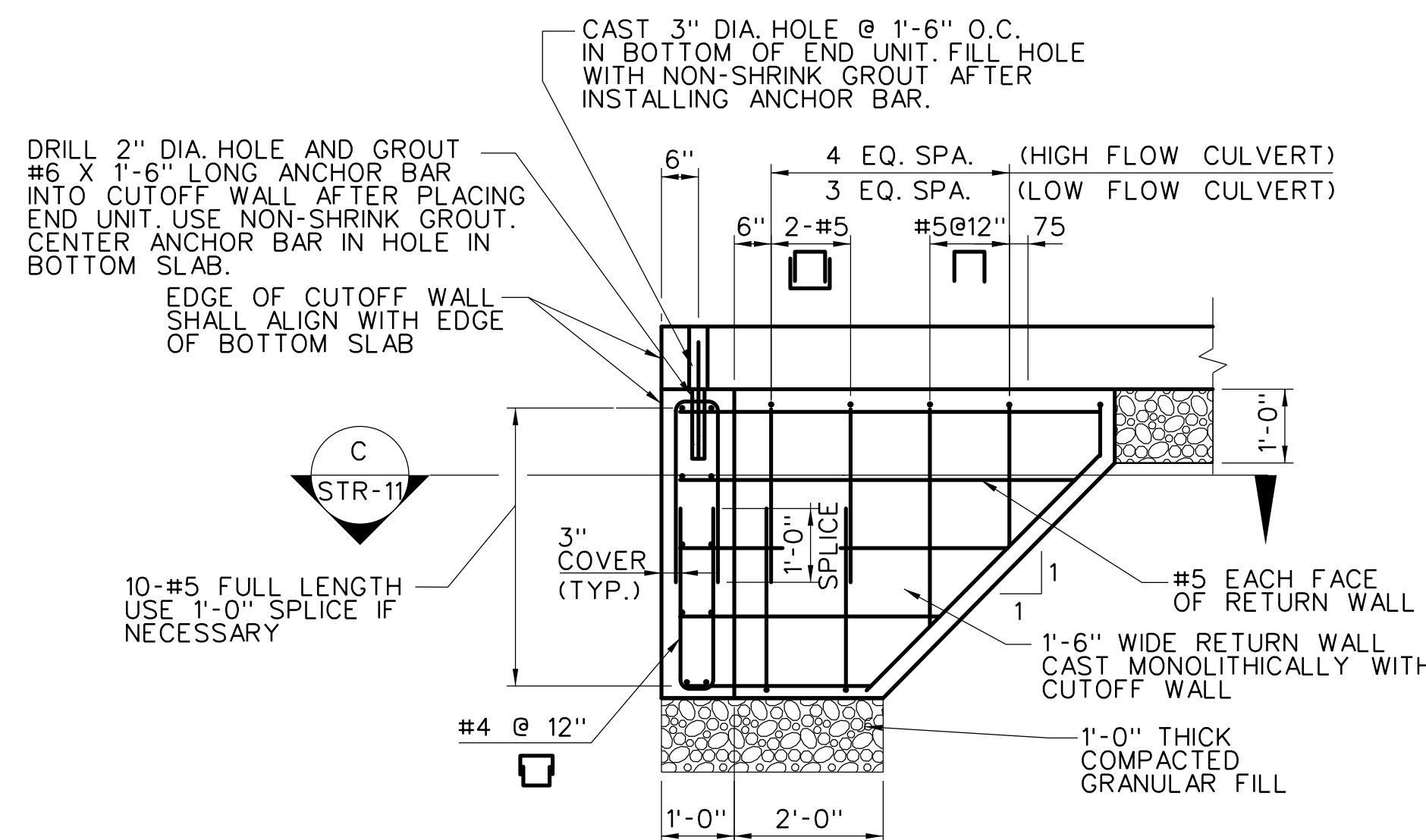




**CULVERT PLAN**  
SCALE: 1/4" = 1'-0"

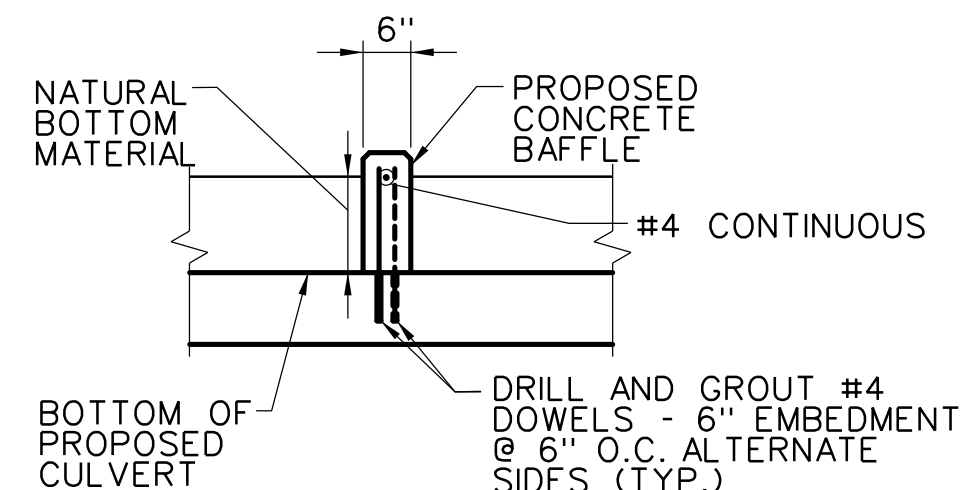


**SECTION A**  
SCALE: 1/2" = 1'-0" STR-11

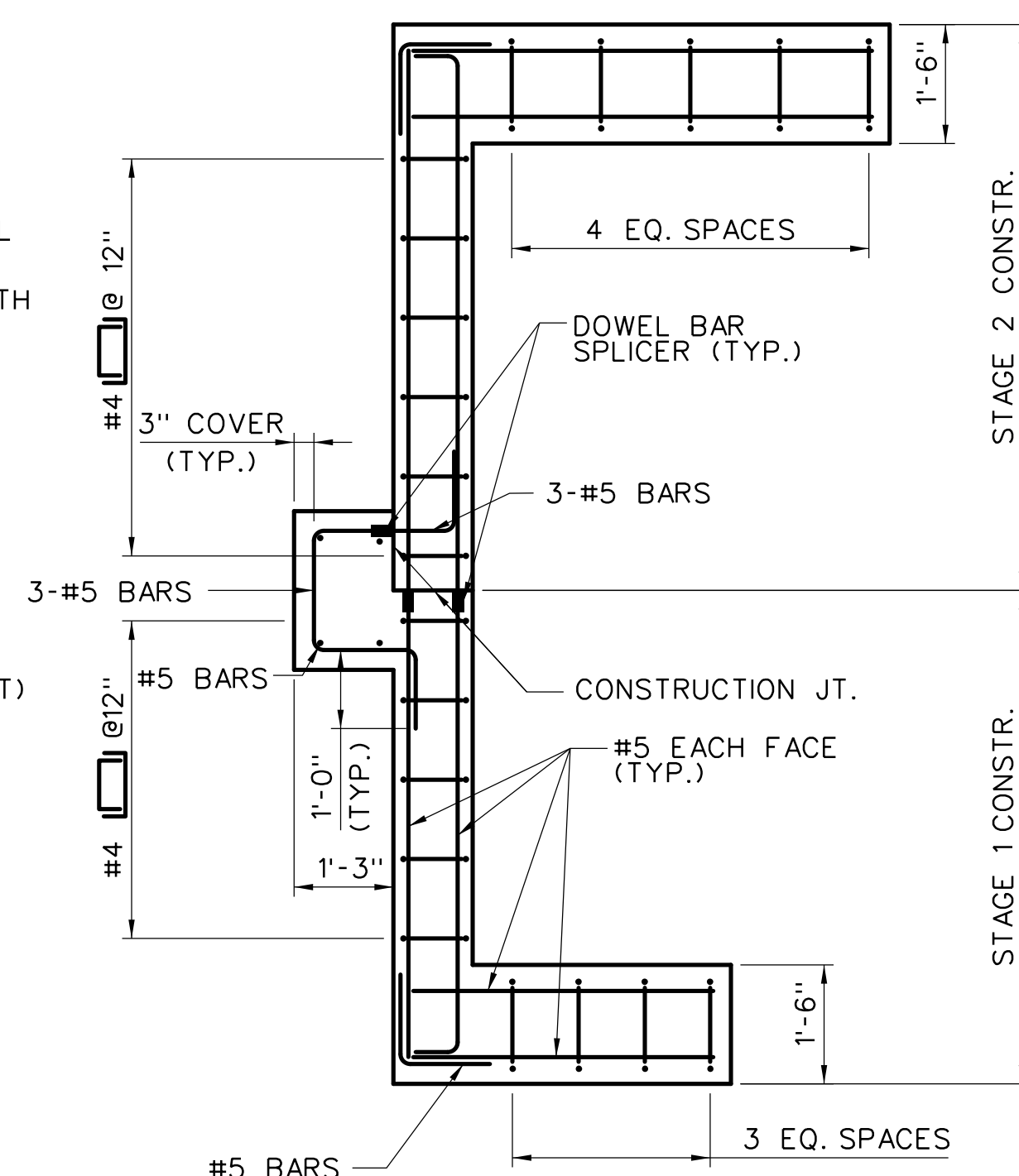


**CUTOFF AND RETURN WALL DETAIL**  
SCALE: 1/2" = 1'-0"

(CUTOFF AND RETURN WALL AT HIGH FLOW CULVERT SHOWN. SIMILAR AT LOW FLOW CULVERT)

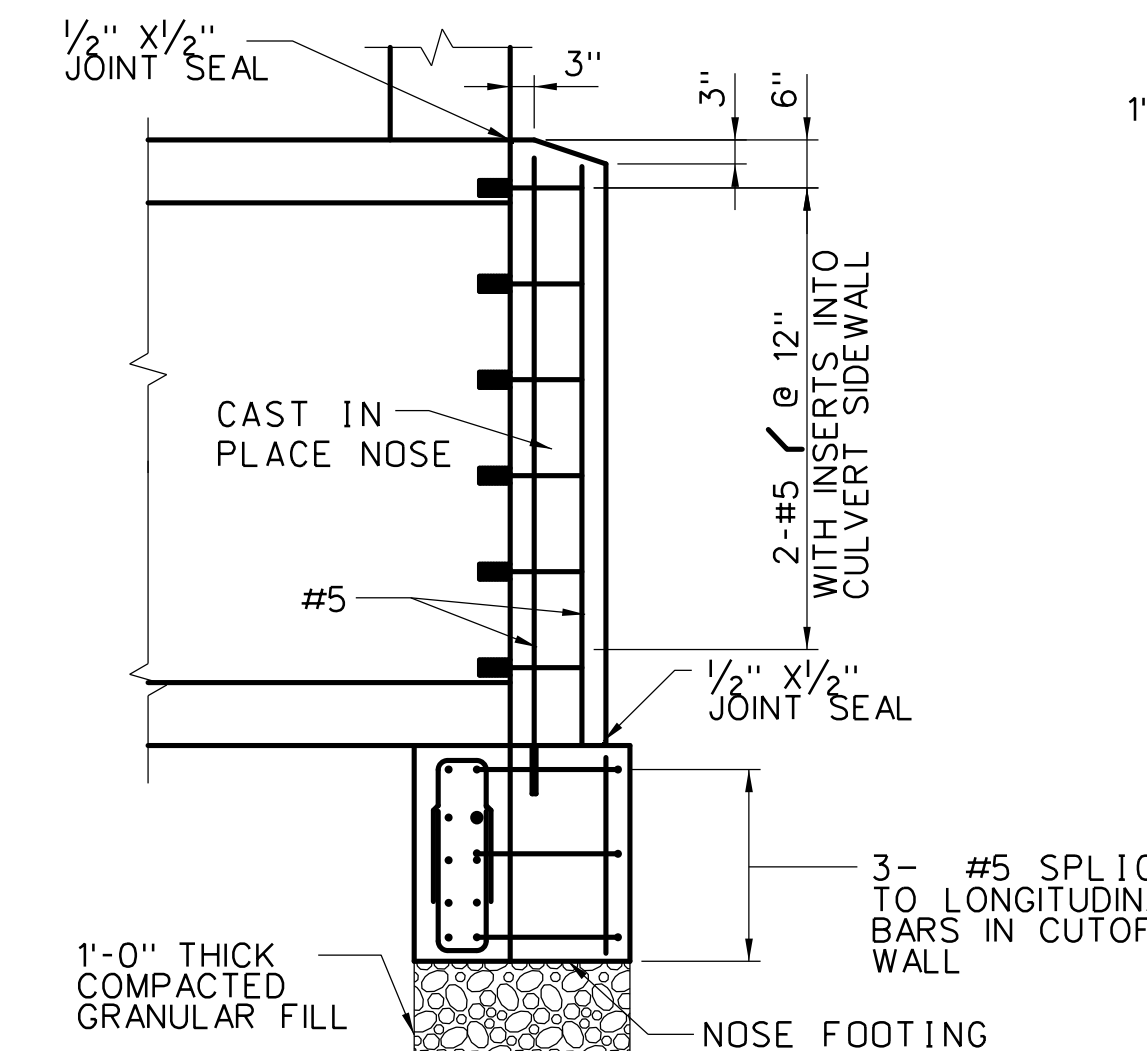


**SECTION B**  
SCALE: 1/2" = 1'-0" STR-11

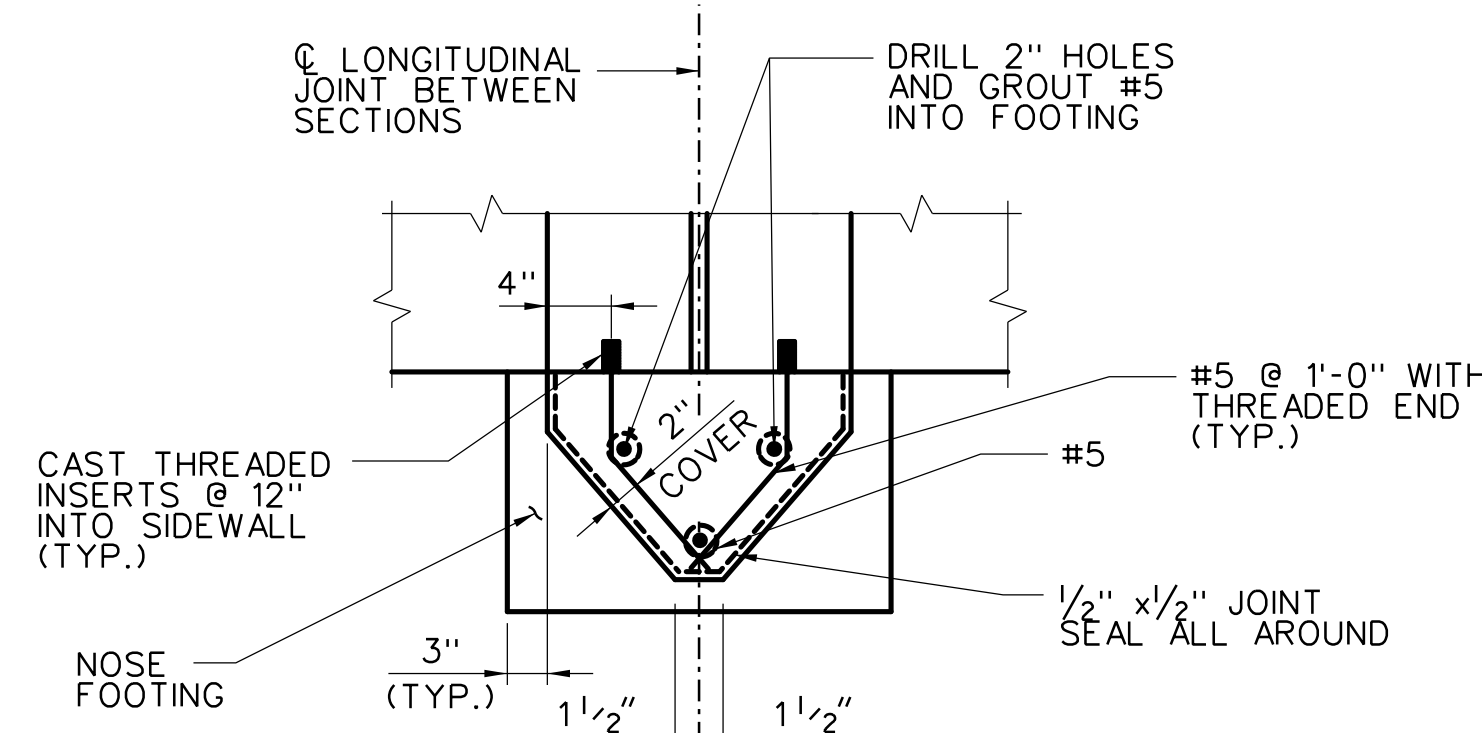


**SECTION C**  
SCALE: 1/2" = 1'-0" STR-11

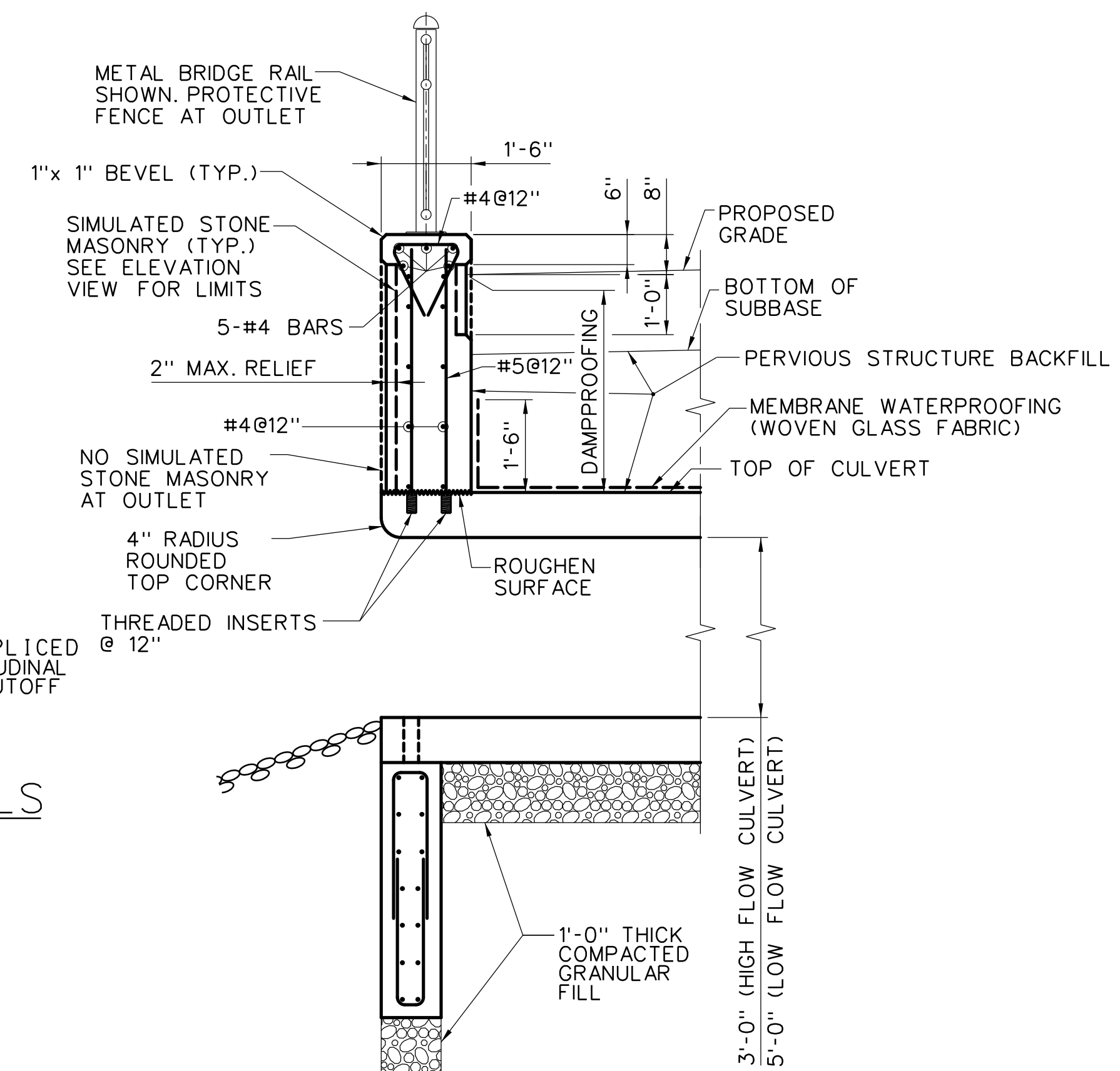
(INLET CUTOFF AND RETURN WALL SHOWN. OUTLET IS OPPOSITE HAND.)



**CAST IN PLACE NOSE DETAILS**  
SCALE: 1/2" = 1'-0"






**CAST IN PLACE NOSE DETAILS**  
SCALE: 1" = 1'-0"



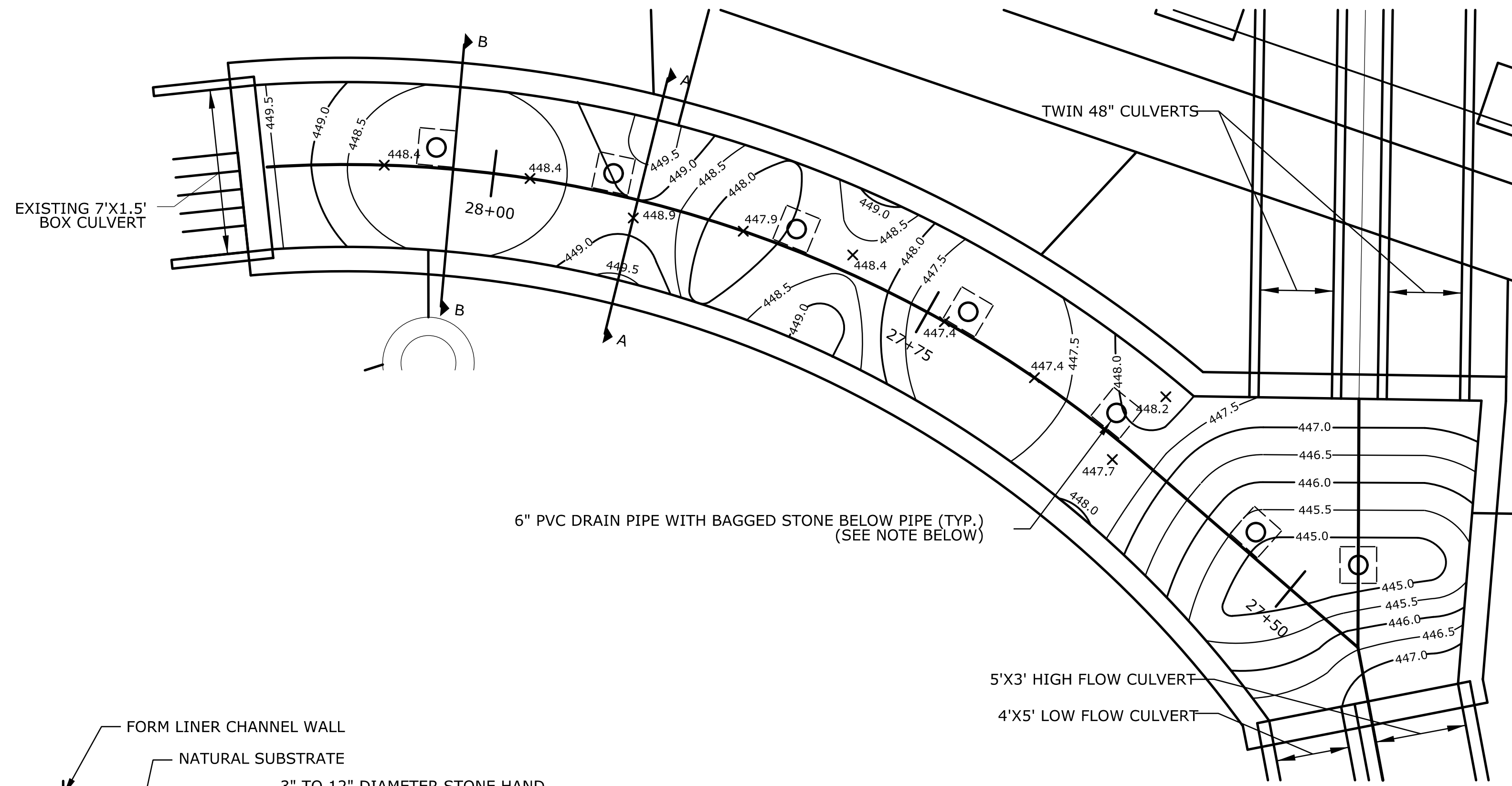
**SECTION F**  
SCALE: 1/2" = 1'-0" STR-7

**NOTES:**

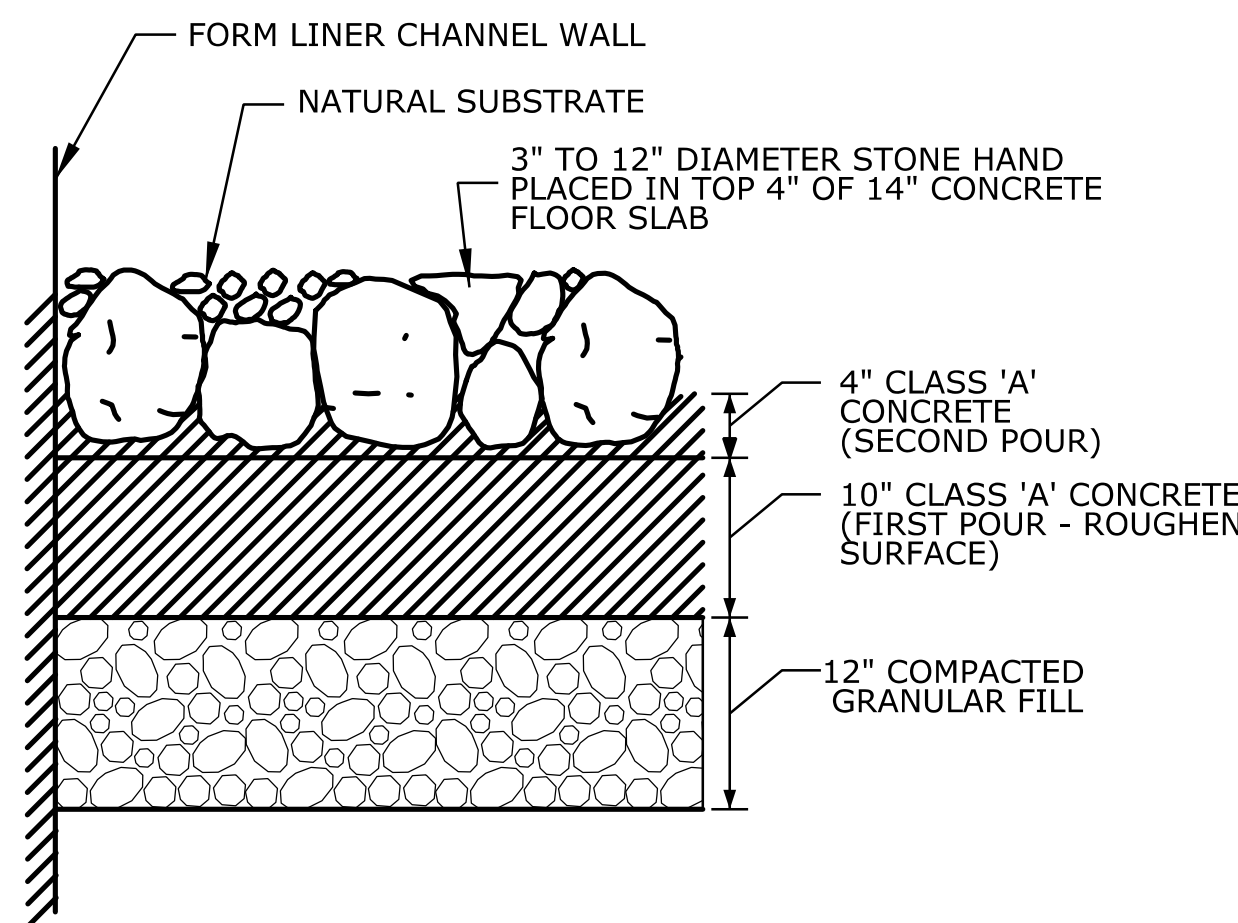
1. THE CONTRACTOR SHALL DESIGN, MANUFACTURE AND CONSTRUCT THE PRECAST BOX CULVERTS IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND THE INSIDE DIMENSIONS, LENGTH AND DETAILS SHOWN ON THESE PLANS.
2. ALL INSERTS OR HOLES CAST INTO THE CULVERT SECTIONS FOR THE SOLE PURPOSE OF HANDLING AND SETTING THE UNITS SHALL BE GROUTED OVER TO A SMOOTH FINISH UPON COMPLETION OF THE WORK.
3. NON-SHRINK GROUT SHALL BE USED TO GROUT THE REINFORCEMENT.
4. THE COST OF FINISHING AND INSTALLING INSERTS SHALL BE INCLUDED IN THE COST OF THE PRECAST CONCRETE BOX CULVERTS AND SHALL BE ONE OF THE FOLLOWING:
  1. STAR EXPANSION INDUSTRIES CORP. TYPE P-35-T
  2. RICHMOND SCREW ANCHOR CO. TYPE LF
  3. DAYTON SUPERIOR CORP. TYPE F-57
 ALL INSERTS SHALL HAVE A CORROSION RESISTANT COATING.
5. THE #4 AND #5 BARS WITH THREADED END SHALL BE COMPATIBLE WITH THE THREADED INSERTS (SEE NOTE 4). THREADS SHALL BE LONG ENOUGH TO FULLY ENGAGE THE INSERTS. THE THREADED BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A615, GRADE 60, AND THE COST SHALL BE INCLUDED IN THE COST OF THE PRECAST CONCRETE BOX CULVERTS.

-	-	-	-	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DESIGNER/DRAFTER: L.PIPPIN/J.GOPAL	 <div>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</div> 		PROJECT TITLE:  RECONSTRUCTION OF ROUTE 31	TOWN:  COVENTRY	PROJECT NO. 32-130	
-	-	-	-								CHECKED BY: L.PIPPIN
-	-	-	-								
-	-	-	-		SCALE AS NOTED	ENGINEER:	BSC GROUP / BY GM2 ASSOCIATES, INC.	DRAWING NO. STR-11			
-	-	-	-			APPROVED BY:	DATE:		SHEET NO. 04.11		
REV.	DATE	REVISION DESCRIPTION	SHEET NO.								

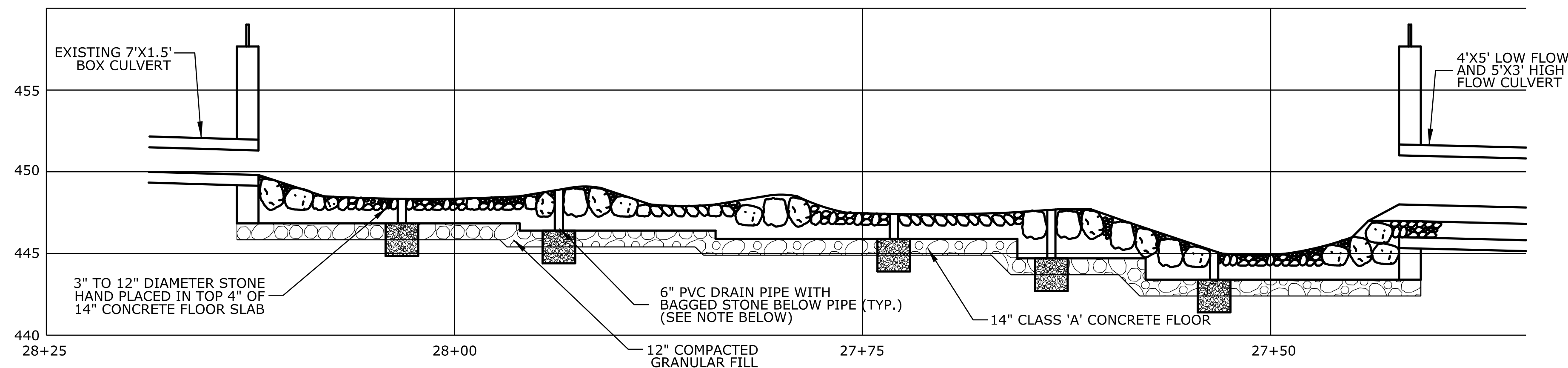




**PAVED CHANNEL PLAN**  
SCALE: 1"=5'

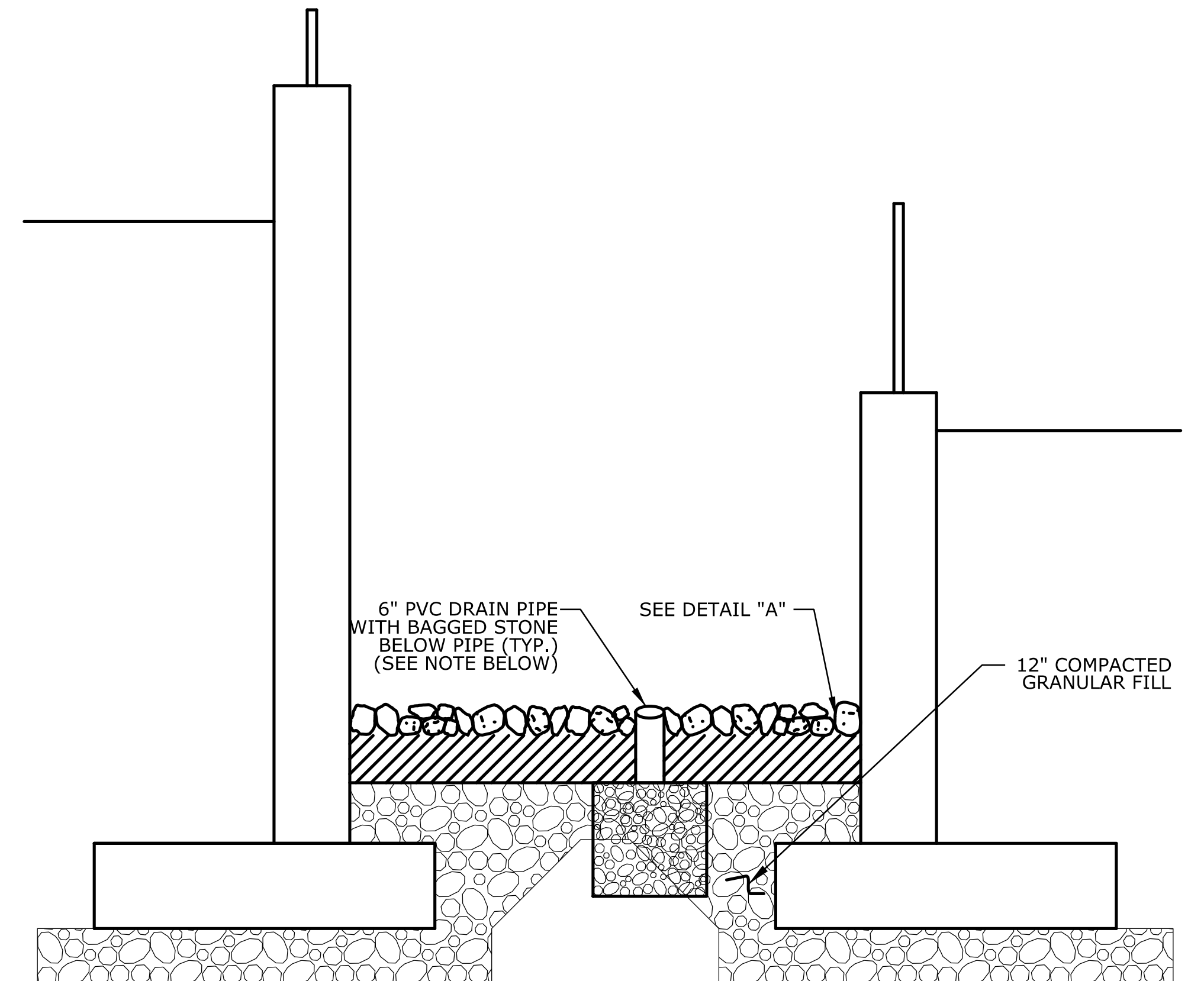
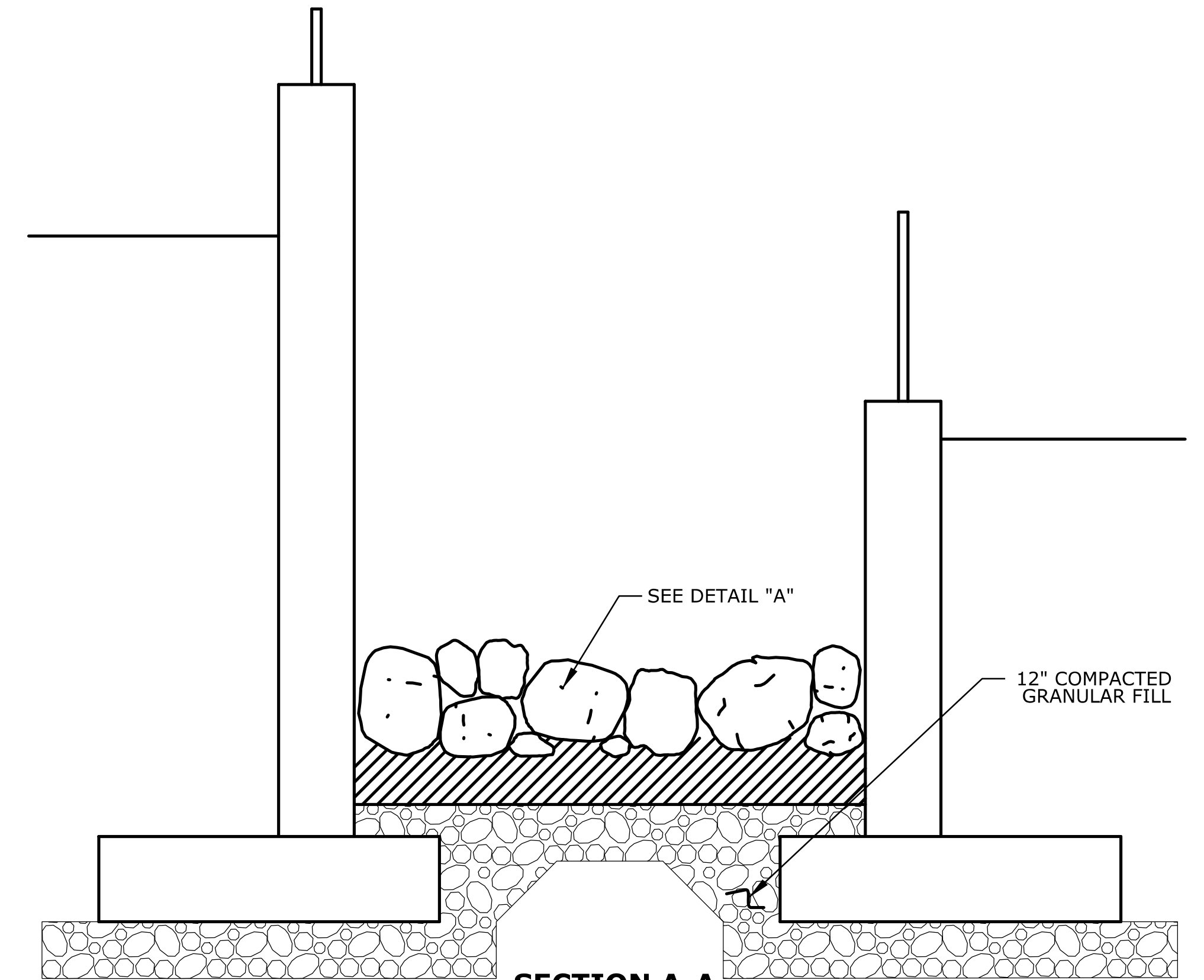


**DETAIL "A"**  
HORIZONTAL SCALE: 1"=1'



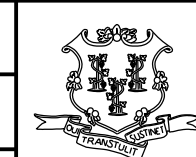
**PAVED CHANNEL PROFILE**  
HORIZONTAL SCALE: 1"=5'  
VERTICAL SCALE: 1"=5'

NOTE: 2' 0" X 2' 0" X 2' 0" BAGGED STONE (8 BAGS) AT EACH 6" PVC DRAIN PIPE



REV.	DATE	REVISION DESCRIPTION	SHEET NO.

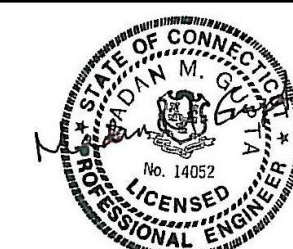
DESIGNER/DRAFTER:  
PJB-MSR / MSR  
CHECKED BY:  
PJB  
SCALE AS NOTED



STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION



ENGINEER: BSC GROUP / BY GM2 ASSOCIATES, INC.  
APPROVED BY: DATE:



PROJECT TITLE:

RECONSTRUCTION OF ROUTE 31

TOWN:

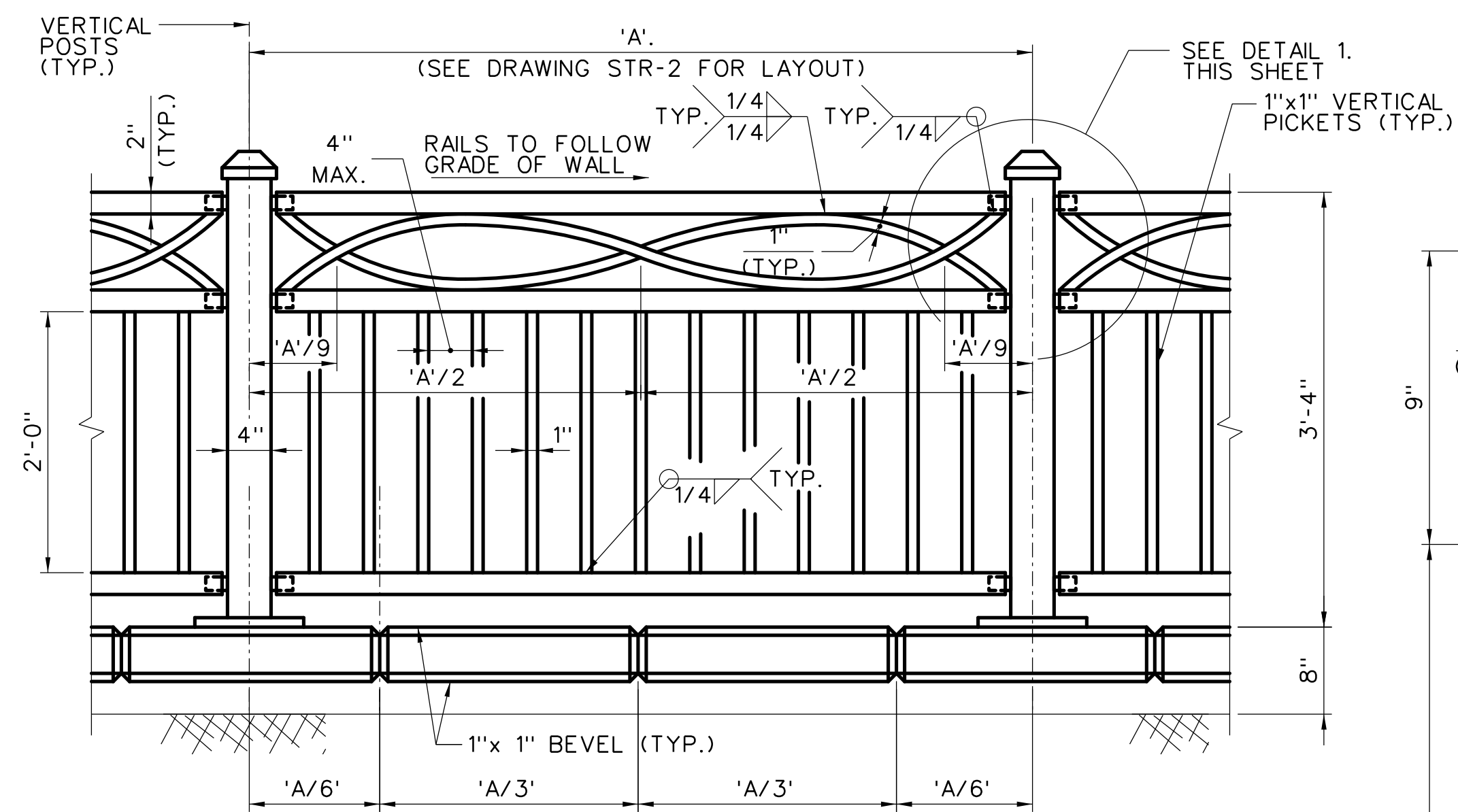
COVENTRY

DRAWING TITLE:

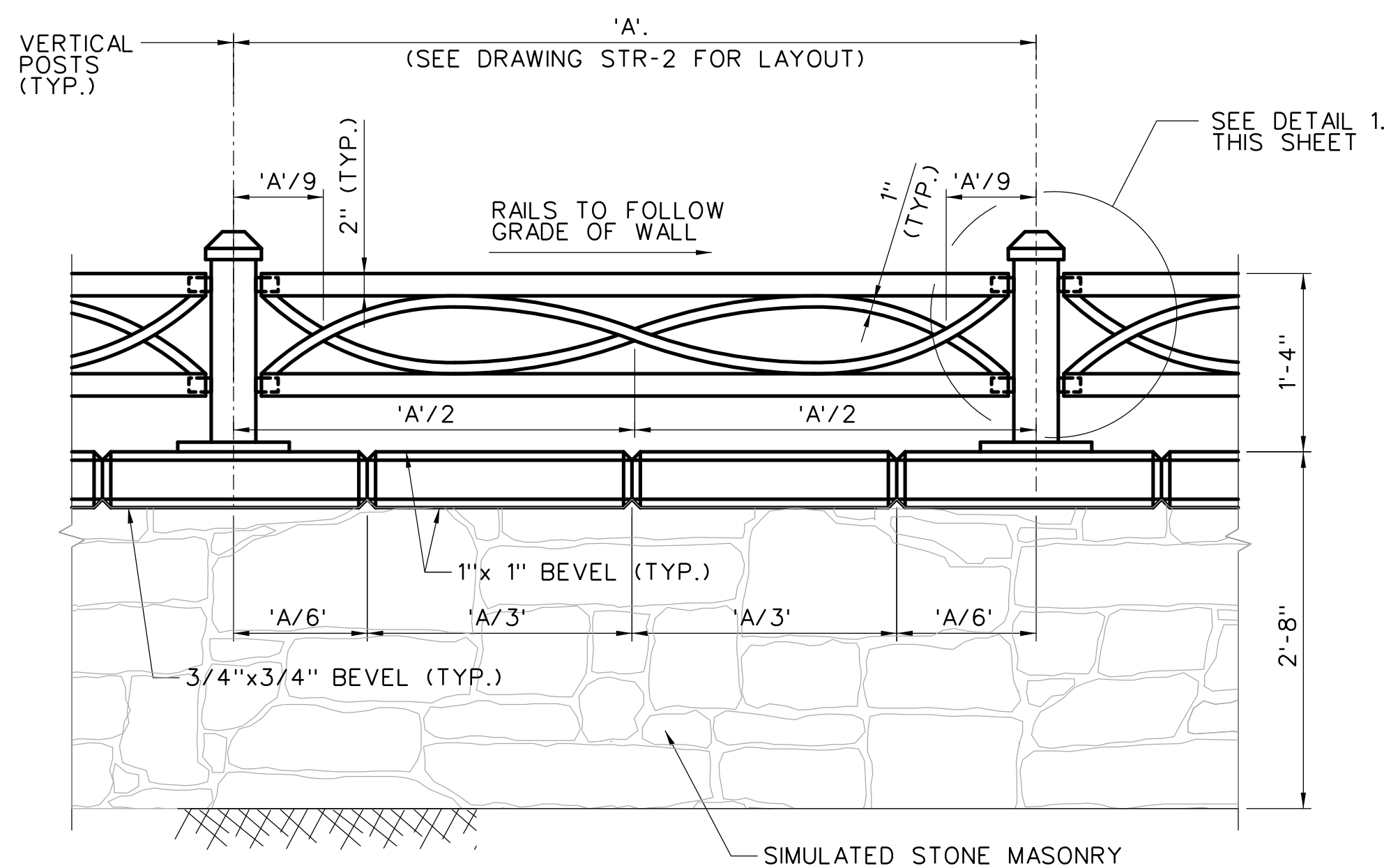
PAVED CHANNEL DETAILS

PROJECT NO.  
**32-130**  
DRAWING NO.  
STR-12  
SHEET NO.  
04.12

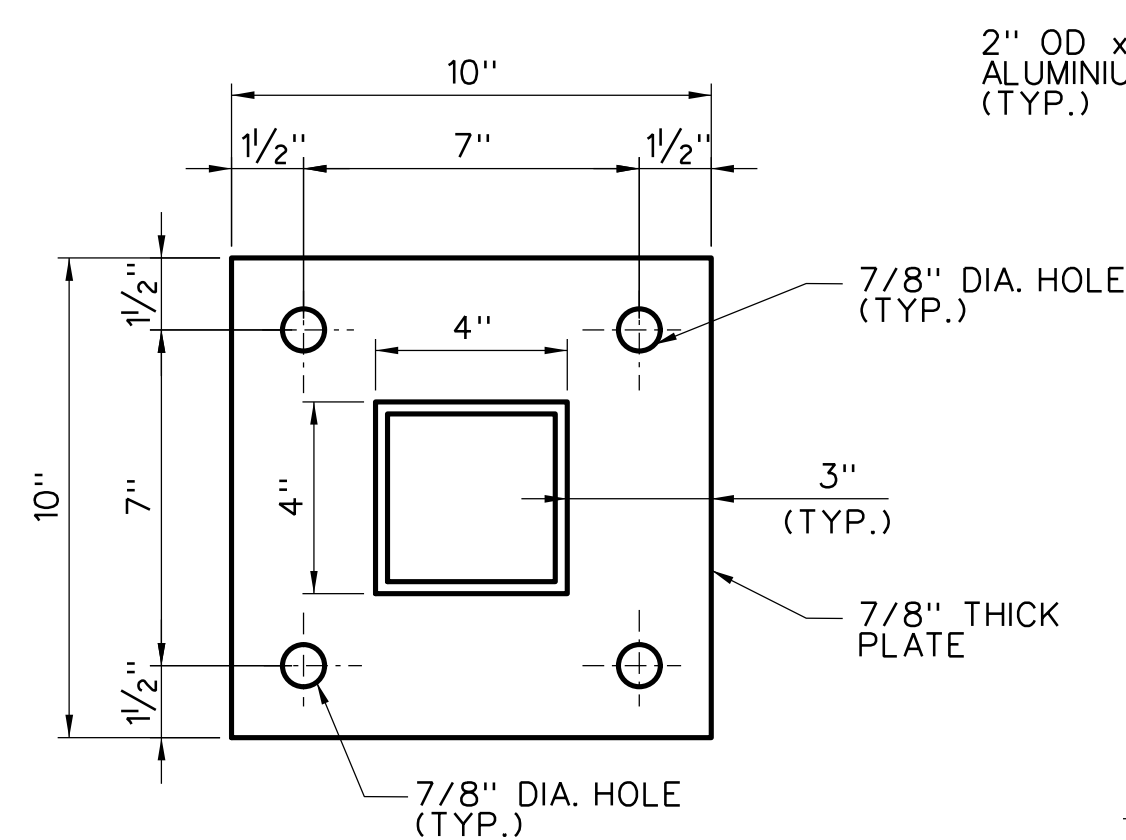




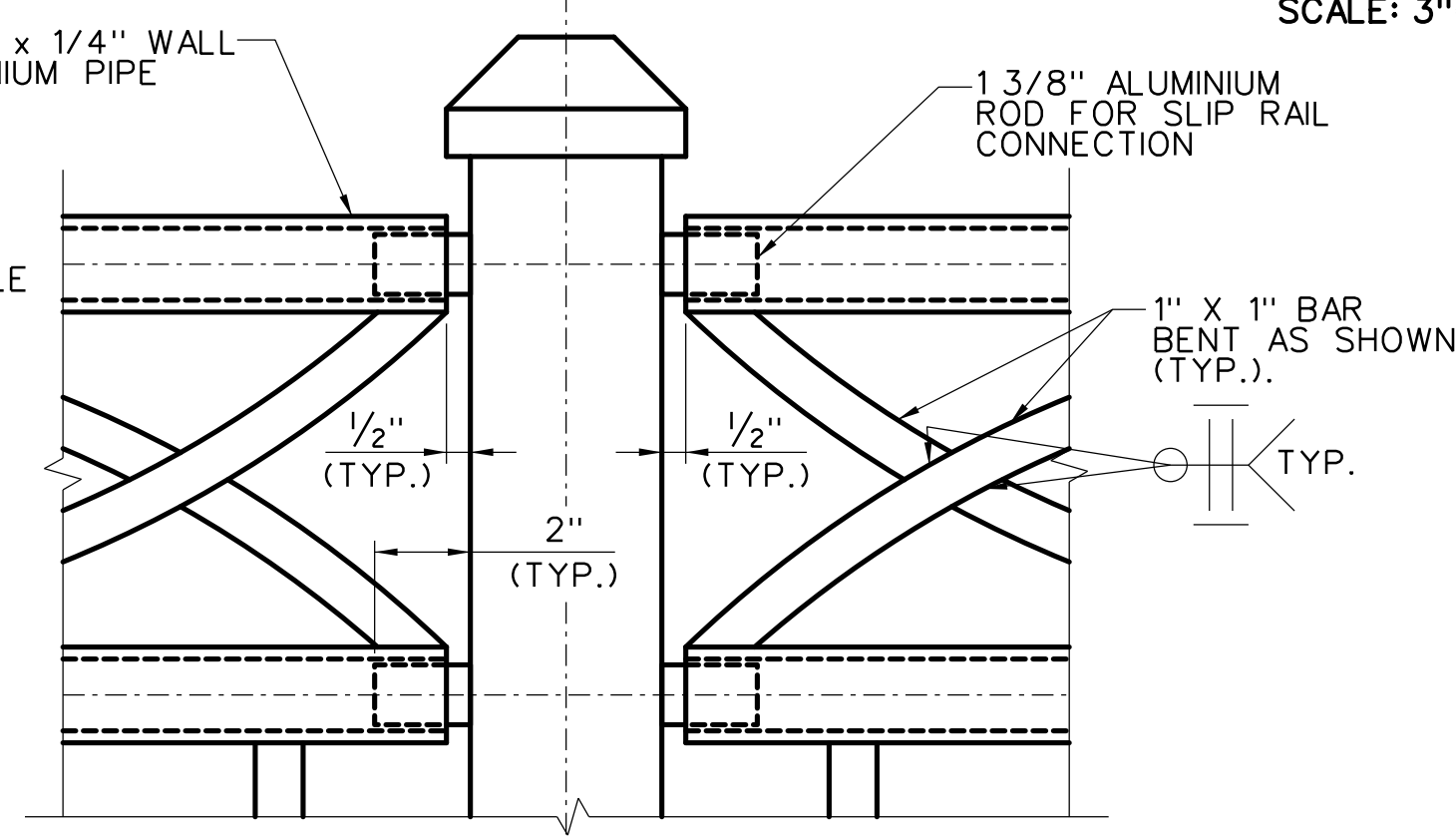
**METAL BRIDGE RAIL (HANDRAIL)**  
SCALE: 1"=1'-0"



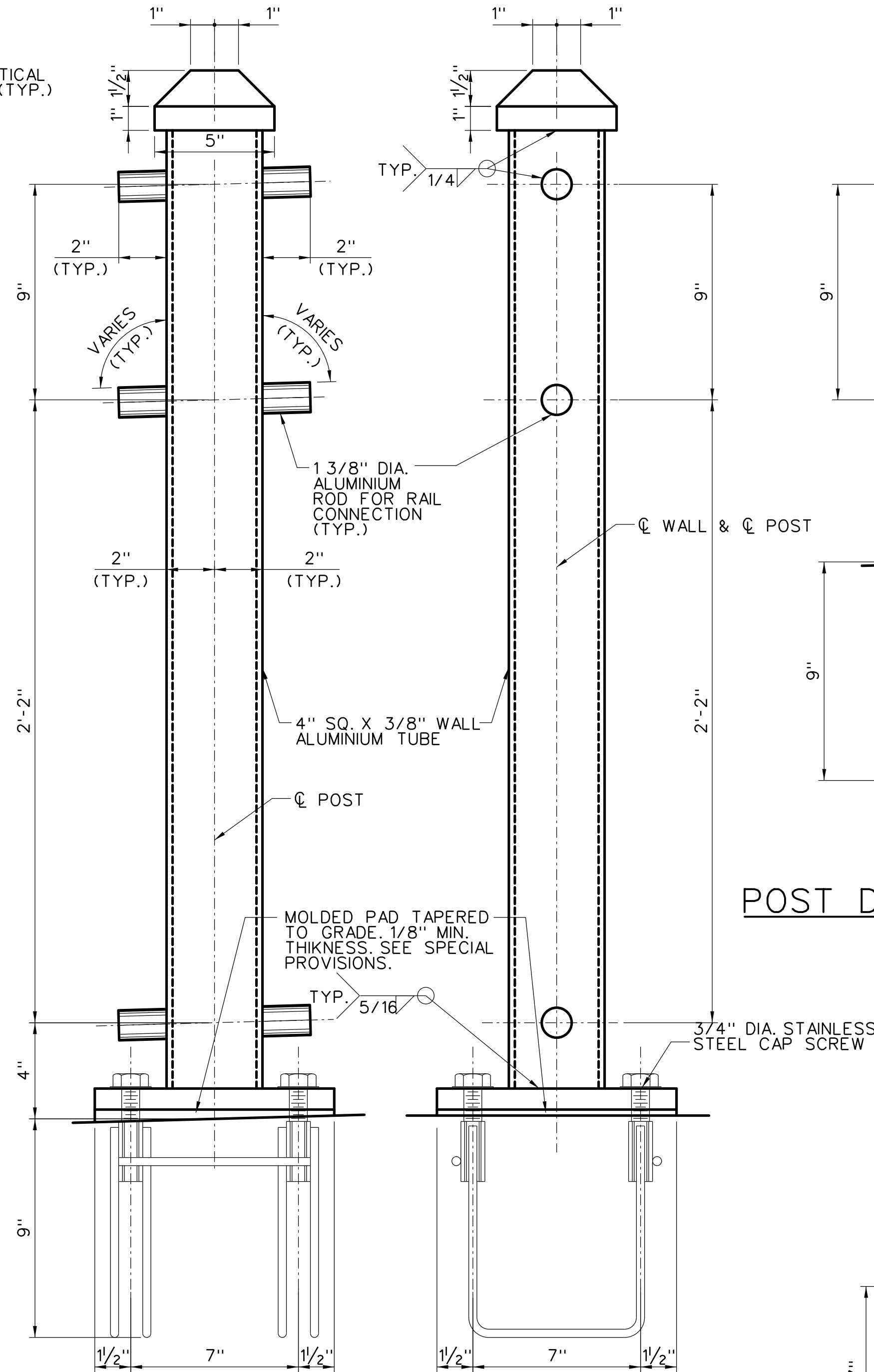
**METAL BRIDGE RAIL (MODIFIED)**  
SCALE: 1"=1'-0"



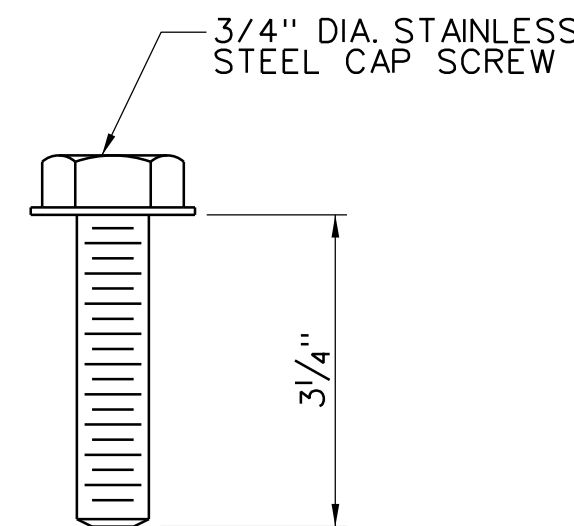
**PLAN - BASE PLATE**  
SCALE: 3"=1'-0"



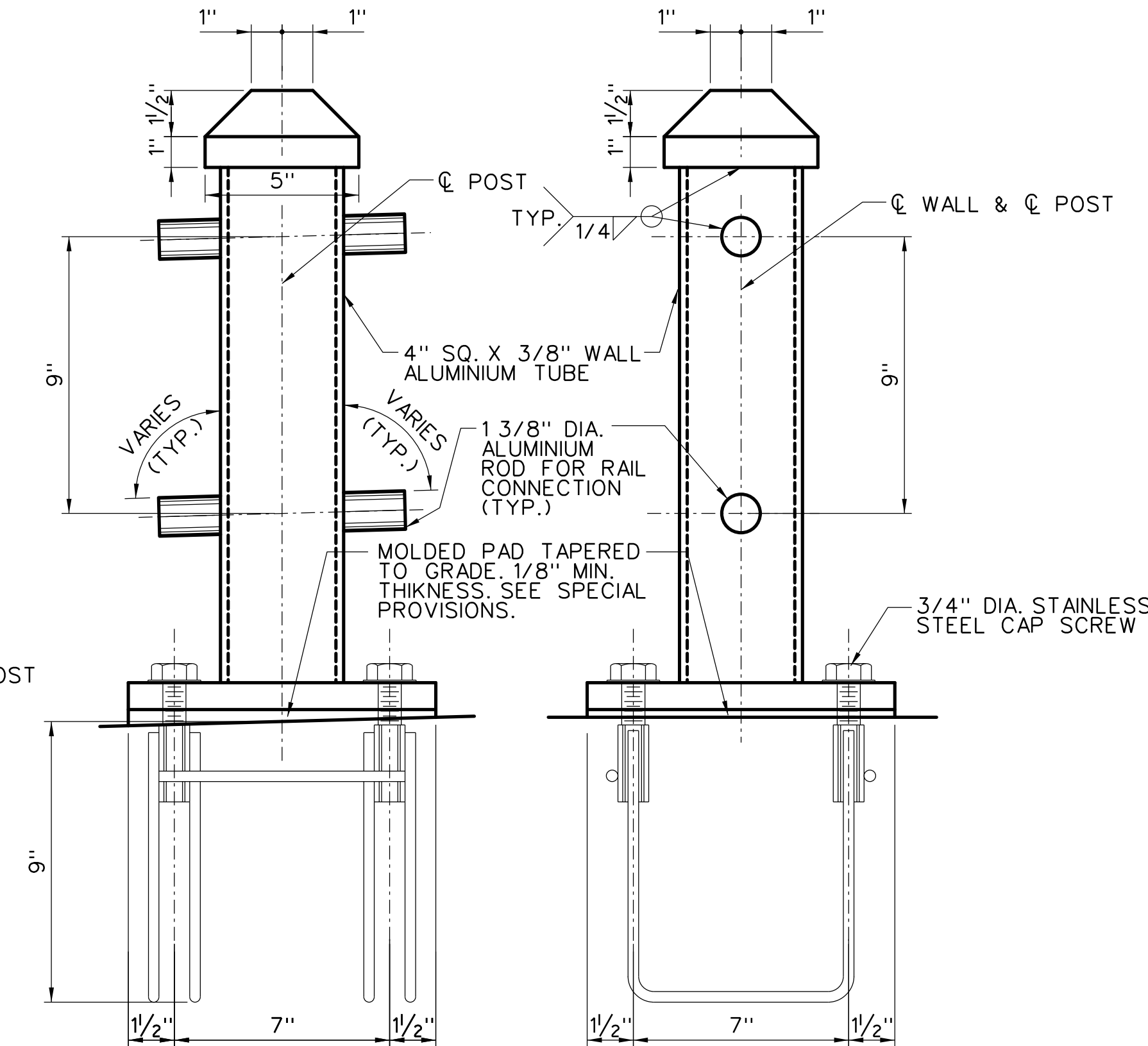
**DETAIL 1**  
SCALE: 3"=1'-0"



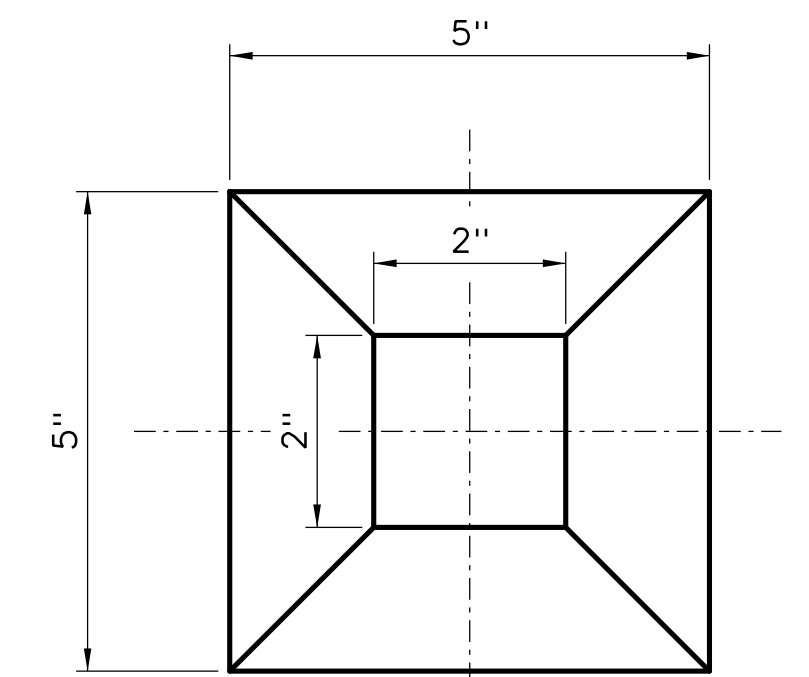
**POST DETAILS - METAL BRIDGE RAIL (HANDRAIL)**  
SCALE: 3"=1'-0"



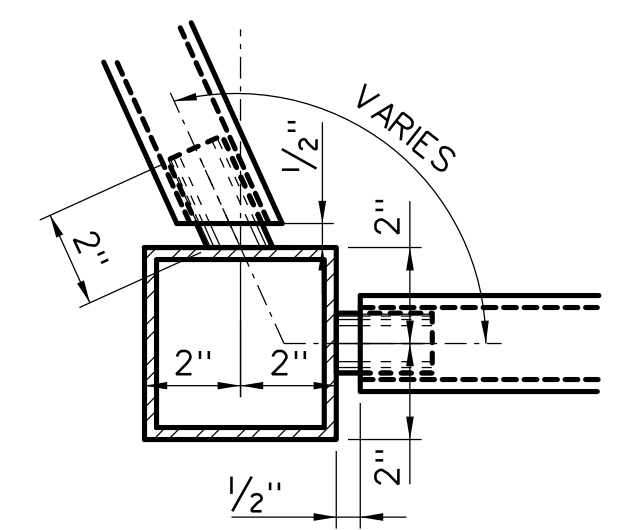
**CAP SCREW**  
N.T.S.



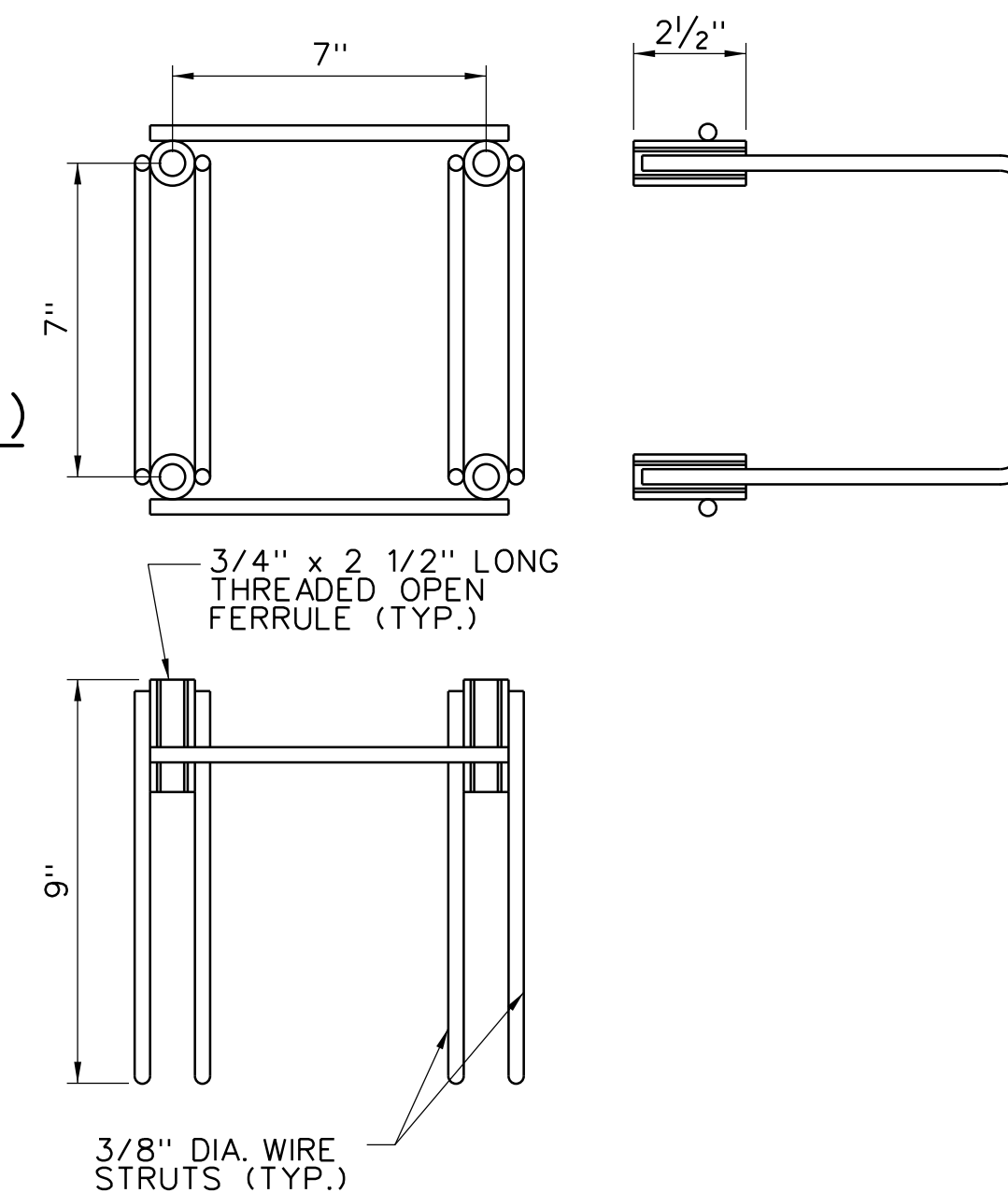
**POST DETAILS - METAL BRIDGE RAIL (MODIFIED)**  
SCALE: 3"=1'-0"



**POST CAP PLAN**  
SCALE: 6"=1'-0"



**POST SECTION AT CORNERS**  
SCALE: 3"=1'-0"



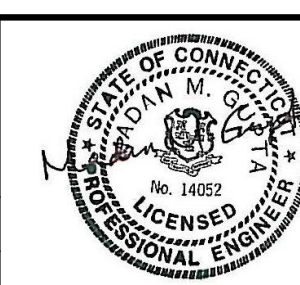
**PRESET ANCHORAGE**  
N.T.S.

#### RAILING NOTES:

- ALL RAILING IS TO BE FABRICATED AND ERECTED SO THAT THE RAILS ARE PARALLEL TO EACH OTHER AND TO THE TOP OF THE WALL. ALL POSTS SHALL BE VERTICAL.
- THE FINISHED RAILING MUST MEET ALL THE REQUIREMENTS OF FIT, ALIGNMENT AND GRADE TO THE SATISFACTION OF THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR FIELD MEASUREMENTS MADE BEFORE ANY SHOP FABRICATION IS PERFORMED.
- ALL RAILING COMPONENTS UNLESS OTHERWISE NOTED SHALL BE EXTRUDED ALUMINUM CONFORMING TO ASTM B221, ALUMINUM ALLOY 6061-T6 AND SHALL BE ANODIZED ACCORDING TO ASTM B580, COATING TYPE A - ENGINEERING HARD COAT. COLOR SHALL BE BLACK.
- EXPOSED SURFACES OF POSTS, RAILING, RINGS, POST CAPS, CONNECTION PLATES AND ANCHORAGE MATERIAL SHALL BE FREE OF BURRS AND SHARP EDGES.
- RAILS SHALL BE SHOP BENT TO REQUIRED RADIIAS DETERMINED FROM LAYOUT PLAN ON DRAWING STR-2.
- THE COST OF FURNISHED AND INSTALLING METAL BRIDGE RAILING SHALL BE INCLUDED IN THE UNIT COST FOR CONTRACT ITEMS "METAL BRIDGE RAIL (HANDRAIL)" AND "METAL BRIDGE RAIL (MODIFIED)".
- ALL BOLTS SHALL BE STAINLESS STEEL AND SHALL CONFORM TO THE REQUIREMENTS OF ASTM A193, CLASS 10R, 2, GRADE B8 STAINLESS STEEL WASHERS SHALL CONFORM TO ASTM A167, TYPES 302-305.

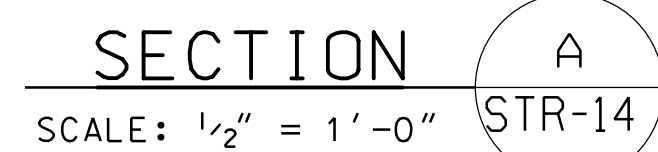
REVISION	DATE	REVISION DESCRIPTION	SHEET NO.
1			
2			
3			
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8			
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DESIGNER/DRAFTER: L.PIPPIN/J.GOPAL	CHECKED BY: L.PIPPIN	SCALE AS NOTED
<b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION		
ENGINEER: BSC GROUP / BY GM2 ASSOCIATES, INC.	APPROVED BY:	DATE:



PROJECT TITLE:  <b>RECONSTRUCTION OF ROUTE 31</b>	TOWN:  <b>COVENTRY</b>	PROJECT NO. <b>32-130</b>
DRAWING TITLE:  <b>METAL BRIDGE RAIL DETAILS</b>		DRAWING NO. <b>STR-13</b>
		SHEET NO. <b>04.13</b>





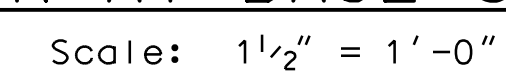
SCALE: 3" = 1'-0"



SCALE: 3" = 1' - 0"



Scale:  $3'' = 1' - 0''$



ALL POSTS SHALL BE INSTALLED PLUMB.

ALL RAILS SHALL BE PARALLEL TO THE TOP OF THE PARAPET.

CHAINLINK FENCE FABRIC SHALL BE #9 GAGE STEEL WIRE,  
GALVANIZED AND PVC-COATED AND WOVEN INTO A 1" MESH.  
THE COLOR SHALL BE BLACK. PVC COATING SHALL BE CLASS 2B,  
THERMALLY FUSED AND BONDED.



STEEL POSTS AND RAILS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A53, TYPE E OR S, GRADE B. THE STEEL POSTS, RAILS AND BASE PLATES SHALL BE HOT-DIP GALVANIZED AFTER FABRICATION. NO WELDING SHALL BE PERFORMED AFTER GALVANIZING.

ANCHOR BOLTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F593, GROUP 1 (AISI TYPE 304). NUTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F594, GROUP 1, AND CIRCULAR WASHERS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A167, TYPES 302 TO 305.

THE LENGTH OF FENCE FOR PAYMENT SHALL BE THE ACTUAL NUMBER OF LINEAR FEET OF CHAIN LINK FENCE, INSTALLED AND ACCEPTED, MEASURED HORIZONTALLY BETWEEN POSTS WITHIN THE LIMITS SHOWN ON PLANS. IT WILL BE PAID FOR UNDER THE ITEM "PROTECTIVE FENCE (5' HIGH)".

MOLDED PADS SHALL BE MANUFACTURED FROM NEW UNVULCANIZED ELASTOMER AND UNUSED SYNTHETIC FIBERS.

FOR ADDITIONAL INFORMATION, SEE SPECIAL PROVISION "PROTECTIVE FENCE (5' HIGH)".

-	-	-	-	-	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DESIGNER/DRAFTER: L.PIPPIN/J.GOPAL	 <b>STATE OF CONNECTICUT</b> DEPARTMENT OF TRANSPORTATION		PROJECT TITLE:  <b>RECONSTRUCTION OF ROUTE 31</b>	TOWN:  <b>COVENTRY</b>	PROJECT NO. <b>32-130</b>
-	-	-	-	-		CHECKED BY: L.PIPPIN				ENGINEER: BSC GROUP / BY GM2 ASSOCIATES, INC.	DRAWING NO. <b>STR-14</b>
-	-	-	-	-		SCALE AS NOTED	APPROVED BY: _____	DATE: _____		SHEET NO. <b>04.14</b>	
REV.	DATE	REVISION DESCRIPTION	SHEET NO.								
Filename:											